



## Notice of Meeting and Meeting Agenda Salt Spring Island Local Community Commission

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Tuesday, July 18, 2023

9:00 AM

SIMS Boardroom  
124 Rainbow Road  
Salt Spring Island BC

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MS Teams Link: [click here](#)

E. Rook (Chair), G. Holman (Vice Chair), G. Baker, B. Corno, B. Webster

The Capital Regional District strives to be a place where inclusion is paramount and all people are treated with dignity. We pledge to make our meetings a place where all feel welcome and respected.

### 1. Call to Order

### 2. Territorial Acknowledgement

### 3. Approval of Agenda

### 4. Adoption of Minutes

#### 4.1. [23-505](#) Minutes of the June 20, 2023 Salt Spring Island Local Community Commission Meeting

**Recommendation:** That the minutes of the Salt Spring Island Local Community Commission of June 20, 2023 be adopted as circulated.

**Attachments:** [Minutes - June 20, 2023](#)

### 5. Chair and Commissioner Remarks

### 6. Presentations/Delegations

*Delegations will have the option to participate electronically. Please complete the online application at [www.crd.bc.ca/address](http://www.crd.bc.ca/address) for "Addressing the Salt Spring Island Local Community Commission" no later than 4:30 pm two days before the meeting and staff will respond with details.*

*Alternatively, you may email your comments on an agenda item to the Salt Spring Island Local Community Commission (LCC) at [saltspring@crd.bc.ca](mailto:saltspring@crd.bc.ca). Requests must be received no later than 4:30 p.m. two calendar days prior to the meeting.*

#### 6.1. [23-507](#) A. Scaglione re: Funding for the Salt Spring Island Abattoir

**Attachments:** [Abey Scaglione - Funding for SSI Abboit Redacted](#)

- 6.2. [23-521](#) R. Williams Re: Supplemental Information Supporting the Fire District Request for a Permanent Seat on the Ganges Harbour Walk Steering Committee.

**Attachments:** [R. Williams Re: Supplemental Information Supporting the Fire District Request f](#)

## 7. Commission Business

- 7.1. [23-506](#) Active Transportation Network Plan

**Recommendation:** That the Salt Spring Island Local Community Commission receive the Active Transportation Plan - Ganges Village for information.

**Attachments:** [Active Transportation Plan - Ganges Village](#)  
[Active Transportation Network Plan](#)  
[Active Transportation Presentation](#)

- 7.2. [23-493](#) Merchant Mews Pathway Design - Additional Funding

**Recommendation:** That the Salt Spring Island Local Community Commission recommends to the Capital Regional District Board that the Salt Spring Island Transportation Service 2023 Capital Plan be amended to increase the budget for the Merchant Mews project by \$16,400 funded from the Capital Reserve Fund.

**Attachments:** [Merchant Mews Pathway](#)

- 7.3. [23-509](#) Portlock Park Site Mater Plan

**Recommendation:** That the Salt Spring Island Local Community Commission (LCC) recommends that the three Landscape Conceptual designs developed by LADR Landscape Architects dated June 23, 2023 be used for further community consultation.

**Attachments:** [Portlock Park Master Plan](#)  
[Attachment 1](#)

- 7.4. [23-510](#) Project Reporting Dashboard April - June 2023

**Attachments:** [Project Reporting Dashboard April - June 2023](#)

- 7.5. [23-511](#) Salt Spring Island Community Transit - Quarter 2 Sales 2023

**Attachments:** [Salt Spring Island Community Transit - Quarter 2 Sales 2023](#)

- 7.6. [23-517](#) 2024 Grants-in-Aid Application and Distribution of Funds

**Recommendation:** That the Salt Spring Island Local Community Commission (LCC) recommends that a Salt Spring Island Grant-in-Aid Eligibility Criteria include an annual deadline of April 30 for Salt Spring Island Grant-in-Aid applications.

**Attachments:** [2024 Grants-in-Aid Application and Distribution of Funds](#)



7.7. [23-513](#) Grants-in-Aid - Copper Kettle

**Recommendation:** That the Salt Spring Island Local Community Commission approve a grant-in-aid to Copper Kettle in the amount of \$5,000.00.

**Attachments:** [Grants-in-Aid - Copper Kettle](#)  
[Appendix A](#)

7.8. Mobrae Bus Shelter

7.9. Additional and Evening Meetings

7.10. Discussion on Services

7.11. Project Updates

7.12. Strategic Planning

7.13. [23-518](#) Ganges Harbour Walk Steering Committee Terms of Reference

**Attachments:** [Ganges Harbour Walk Steering Committee Terms of Reference](#)

## 8. Notice(s) of Motion

*Commissioner Corno proposed the following Notice of Motion:*

*That the Salt Spring Island Local Community Commission adopts the following commitment to open government:*

*The Salt Spring Island Local Community Commission will work on an ongoing basis to be accessible, transparent, accountable and open to community advice and guidance.*

### *Accessibility*

*We will be accessible to our community by:*

- *Holding meetings at times and locations that make them as accessible to community members as possible,*
- *Making information available to the community in advance on what topics we will be discussing, where and when, and*
- *Exploring the feasibility of making our meetings – and recordings of them – available via video.*

### *Transparency and Accountability*

*We will work actively to be transparent and accountable to our community by:*

- *Minimizing the use of closed meetings, as guided by the relevant Provincial legislation,*
- *Sharing information widely on the priorities we identify,*
- *Reporting to the community on a regular basis about our progress, and*
- *Holding a reporting and accountability session at least once yearly to report to the community on our activities and receive feedback on how we are doing.*

### *Community Advice and Guidance*

*We commit to welcoming and considering public advice and guidance. We invite Salt Spring residents to provide this by:*

- *Sending us e-mails or letters,*
- *Speaking as a delegation at an LCC meeting,*
- *Inviting LCC members to attend community meetings and other gatherings.*

## 9. Correspondence

- 9.1.      [23-514](#)      Capital Regional District Board Engagement on Transportation Governance

**Attachments:**      [Capital Regional District Board Engagement on Transportation Governance](#)

## 10. Motion to Close the Meeting

*That the meeting be closed in accordance with the Community Charter, Part 4, Division 3, 90 (a) personal information about an identifiable individual who holds or is being considered for a position as an officer, employee or agent of the municipality or another position appointed by the municipality;*

## 11. Rise and Report

### 13. Adjournment

*The next meeting is TBD, at 9:00AM in the Salt Spring Island Multi Space (SIMS) Boardroom, 124 Rainbow Road, Salt Spring Island, BC V8K 2V5.*

*To ensure quorum, please advise Shayla Burnham 250 537 4448 if you cannot attend.*



Making a difference...together

**Minutes of the Inaugural Meeting of the Salt Spring Island Local Community Commission  
Held Tuesday, June 20, 2023 at the Salt Spring Island Multi-Space (SIMS)  
124 Rainbow Rd, Salt Spring Island, BC V8K 2K3**

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**DRAFT**

**Present:**       **Director:** Gary Holman (Vice Chair)  
                  **Commission Members:** Earl Rook (Chair), Gayle Baker, Ben Corno and  
                  Brian Webster  
                  **Staff:** Ted Robbins, Chief Administrative Officer, Kristen Morley, General  
                  Manager, Corporate Services, Karla Campbell, Senior Manager, Salt Spring Island  
                  Administration, Dan Ovington, Parks and Recreation Manager, Dean Olafson,  
                  Engineering Manager, Salt Spring Island, and Shayla Burnham,  
                  Recording Secretary

These minutes follow the order of the agenda although the sequence may have varied.

**1. Call to Order**

The meeting was called to order at 9:01am by Ted Robbins, Chief Administrative Officer.

**2. Territorial Acknowledgement / Call Meeting to Order**

A Territorial Acknowledgement was provided by Ted Robbins, Chief Administrative Officer.

**3. Oath of Office**

Kristen Morley, General Manager, Corporate Services, swore in Gayle Baker, Ben Corno, Earl Rook and Brian Webster.

**4. Election of Chair**

Ted Robbins, Chief Administrative Officer, called for nominations for Chair. Commissioner Baker elected Commissioner Rook for Chair. After calling three times and hearing no other nominations, Commissioner Rook was elected as Chair.

**5. Election of Vice Chair**

Commissioner Rook called for nominations for Vice Chair. Commissioner Baker elected Director Holman for Vice Chair. After calling three times and hearing no other nominations, Director Holman was elected as Vice Chair.

**6. Approval of Agenda**

**MOVED** by Commissioner Rook, **SECONDED** by Commissioner Corno,  
That the Salt Spring Island Local Community Commission approve the amended agenda  
of June 20, 2023 by updating Item 8. Chairs Remark's to "Chair, Director, and  
Commissioner's Remarks" and, by adding Item 12.1 Determine Next Meeting Date.  
Lastly, to add Item 12.2 Active Transportation Grant.

**CARRIED**

**7. Adoption of Minutes**

**7.1 Minutes of October 24, 2022 Salt Spring Island Community Economic  
Sustainability Commission Meeting**

**MOVED** by Director Holman, **SECONDED** by Commissioner Corno,  
That the minutes of the October 24, 2023 meeting be adopted as presented.

**CARRIED**

**7.2 Minutes of March 14, 2023 Salt Spring Island Parks and Recreation  
Commission Meeting**

**MOVED** by Director Holman, **SECONDED** by Commissioner Corno,  
That the minutes of the March 14, 2023 meeting be adopted as presented.

**CARRIED**

**7.3 Minutes of March 27, 2023 Salt Spring Island Transportation Commission  
Meeting**

**MOVED** by Director Holman, **SECONDED** by Commissioner Corno,  
That the minutes of the March 27, 2023 meeting be adopted as presented.

**CARRIED**

The Commission expressed that the above minutes were those of dissolved  
Commissions and that they were not present during the meetings.

**8. Chair, Director, and Commissioner's Remarks**

**Chair Rook** briefly reported:

- Thanked the assembled members of the public for attending the inaugural meeting.

**Director Holman** briefly reported:

- Expressed congratulations to the other LCC Commissioners and public.
- Significant budget pressures for the 2024 LCC Commission forthcoming.

**Commissioner Baker** briefly reported:

- Expressed congratulations to the other LCC Commissioners and public.

**Commissioner Corno** briefly reported:

- Encouraged the community to connect on island priorities and/or issues.

**Commissioner Webster** briefly reported:

- Resigned from prior positions on not-for-profit organizations.

## **9. Presentations/Delegations**

**MOVED** by Director Holman, **SECONDED** by Commissioner Webster,  
That the Salt Spring Island Local Community Commission accept Robin Williams late delegation request regarding the Salt Spring Island Fire Rescue Appointment Request to Ganges Harbour Walk Steering Committee.

**CARRIED**

### **9.1 R. Williams re: Salt Spring Island Fire Rescue Appointment Request to Ganges Harbour Walk Steering Committee**

- R. Williams appointed by the Salt Spring Island Fire Rescue (SSIFR) on Monday, June 19, 2023 to be the Liaison between SSIFR and the LCC.
- SSIFR is requesting an additional member of the SSIFR be appointed to the Ganges Harbour Walk Steering Committee.
- Fire prevention measures, storage and access to waterfront and pumping capacity noted as the location is within the SSIFR fire protection area.

## **10. Commission Business**

### **10.1. Orientation**

- Staff presented an Orientation PowerPoint Presentation that was circulated to the Commission for review.

There is no recommendation. This presentation is for information only.

The Commission called for a break at 10:56am.

The Commission returned to the meeting at 11:05am.

### **10.2. Appoint Members to Ganges Harbour Walk Steering Committee**

Commissioner Rook called for nominations to the Ganges Harbour Walk Steering Committee. Commissioner Baker nominated Commissioner Webster. After calling three times and hearing no other nominations, Commissioner Webster was appointed to the Ganges Harbour Walk Steering Committee.

**CARRIED**

**MOVED** by Commissioner Webster, **SECONDED** by Commissioner Baker,  
That the Salt Spring Island Local Community Commission request an agenda item  
to reassess the composition of the terms of reference for the Ganges Harbour Walk  
Steering Committee be added to the July 18, 2023 Regular meeting agenda for  
discussion.

**CARRIED**

**11. Notice(s) of Motion**

Director Holman proposed the following Notice of Motion for same day consideration:

“That the Salt Spring Island Local Community Commission requests staff to provide a  
report by the next Regular meeting on options to achieve:

1. Holding each monthly regular LCC meeting over two sessions, each to take place  
on a different day,
2. One of these monthly sessions to be held during the day and the other to be held  
in the early evening, and
3. Holding town halls and similar meetings on an as-required basis.”

**MOVED** by Director Holman, **SECONDED** by Commissioner Baker,  
That the same day consideration be applied to the motion.

**CARRIED**

**MOVED** by Director Holman, **SECONDED** by Commissioner Baker  
That the Salt Spring Island Local Community Commission requests staff to provide a report  
by the next Regular meeting on options to achieve:

1. Holding each monthly regular LCC meeting over two sessions, each to take place  
on a different day,
2. One of these monthly sessions to be held during the day and the other to be held  
in the early evening, and
3. Holding town halls and similar meetings on an as-required basis.

**CARRIED**

Commissioner Baker proposed the following Notice of Motion for same day consideration:

“That the the Salt Spring Island Local Community Commission requests staff to advertise  
all future Commission meetings as required under the Board Procedures Bylaw and also  
through postings submitted well in advance to:

1. The “What’s On” section of the Gulf Islands Driftwood newspaper.
2. The “Events” section of Salt Spring Exchange, and
3. Any other low or no-cost forms of advertising staff or LCC members may identify  
that reach significant numbers of Salt Spring Island residents.”

**MOVED** by Commissioner Baker, **SECONDED** by Direction Holman,  
That the same day consideration be applied to the motion.

**CARRIED**

**MOVED** by Commissioner Baker, **SECONDED** by Director Holman,  
That the Salt Spring Island Local Community Commission requests staff to advertise all future Commission meetings as required under the Board Procedures Bylaw and also through postings submitted well in advance to:

1. The “What’s On” section of the Gulf Islands Driftwood newspaper.
2. The “Events” section of Salt Spring Exchange, and

Any other low or no-cost forms of advertising staff or LCC members may identify that reach significant numbers of Salt Spring Island residents.

**CARRIED**

Commissioner Baker proposed the following Notice of Motion for same day consideration:

“That the Salt Spring Island Local Community Commission requests that staff invite the following people to attend our next meeting to provide input on Salt Spring Island Local Community Commission services and strategic priorities and to provide any written documentation and input in advance, if they so desire:

1. Past members of Salt Spring Island advisory commissions, and
2. Salt Spring residents who have expertise in local government and local community commissions.”

**MOVED** by Commissioner Baker, **SECONDED** by Direction Holman,  
That the same day consideration be applied to the motion.

**CARRIED**

**MOVED** by Commissioner Baker, **SECONDED** by Director Holman,  
That the Salt Spring Island Local Community Commission requests that staff invite the following people to attend our next meeting to provide input on Salt Spring Island Local Community Commission services and strategic priorities and to provide any written documentation and input in advance, if they so desire:

3. Past members of Salt Spring Island advisory commissions, and
4. Salt Spring residents who have expertise in local government and local community commissions.

**CARRIED**



Commissioner Webster proposed the following Notice of Motion for same day consideration:

“That that the Salt Spring Island Local Community Commission expresses its thanks for contributions toward establishing our Salt Spring Island Local Community Commission (LCC) to:

- Salt Spring community members who voted to establish the LCC and whose commitment to improved democratic representation and local decision-making led to its creation.
- Past members of Salt Spring advisory commissions who accomplished so much working selflessly over many years under the previous governance system.
- Our Electoral Area Director, whose advocacy for the initiative was essential to it moving forward.
- The CRD Board and its Chair, who supported the right of Salt Spring residents to decide how to govern themselves, and
- CRD staff, including the Salt Spring Island Senior Manager, the Chief Administrative Officer (current and previous) and — particularly — the General Manager of Corporate Services, whose work enabled the Salt Spring Island Community to move in this exciting new direction.”

**MOVED** by Commissioner Webster, **SECONDED** by Commissioner Corno,  
That the same day consideration be applied to the motion.

**CARRIED**

**MOVED** by Commissioner Webster, **SECONDED** by Commissioner Baker,  
That the Salt Spring Island Local Community Commission expresses its thanks for contributions toward establishing our Salt Spring Island Local Community Commission (LCC) to:

- Salt Spring community members who voted to establish the LCC and whose commitment to improved democratic representation and local decision-making led to its creation.
- Past members of Salt Spring advisory commissions who accomplished so much working selflessly over many years under the previous governance system.
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- The CRD Board and its Chair, who supported the right of Salt Spring residents to decide how to govern themselves, and
- CRD staff, including the Salt Spring Island Senior Manager, the Chief Administrative Officer (current and previous) and — particularly — the General Manager of Corporate Services, whose work enabled the Salt Spring Island Community to move in this exciting new direction.

**CARRIED**

Commissioner Webster proposed the following Notice of Motion for same day consideration:

“That the Salt Spring Island Local Community Commission request staff to report back on their request to add a public participation section topic to be included on the agenda to every regular meeting.”

**MOVED** by Commissioner Webster, **SECONDED** by Commissioner Corno,  
That the same day consideration be applied to the motion.

**CARRIED**

**MOVED** by Commissioner Webster, **SECONDED** by Commissioner Corno,  
That the Salt Spring Island Local Community Commission request staff to report back on their request to add a public participation section topic to be included on the agenda to every regular meeting.

**CARRIED**

Director Holman proposed the following Notice of Motion for same day consideration:

“That the Salt Spring Island Local Community Commission request the Chair and Vice Chair to meet with staff to develop agendas and to participate in regular administrative briefings.”

**MOVED** by Director Holman, **SECONDED** by Commissioner Rook,  
That the same day consideration be applied to the motion.

**CARRIED**

**MOVED** by Chair Rook, **SECONDED** by Director Holman,  
That the Salt Spring Island Local Community Commission request the Chair and Vice Chair to meet with staff to develop agendas and to participate in regular administrative briefings.

**CARRIED**

Commissioner Corno proposed the following Notice of Motion:

“That the Salt Spring Island Local Community Commission adopts the following commitment to open government:

The Salt Spring Island Local Community Commission will work on an ongoing basis to be accessible, transparent, accountable and open to community advice and guidance.

**Accessibility**

We will be accessible to our community by:

- Holding meetings at times and locations that make them as accessible to community members as possible,
- Making information available to the community in advance on what topics we will be discussing, where and when, and
- Exploring the feasibility of making our meetings – and recordings of them – available via video.

**Transparency and Accountability**

We will work actively to be transparent and accountable to our community by:

- Minimizing the use of closed meetings, as guided by the relevant Provincial legislation,
- Sharing information widely on the priorities we identify,
- Reporting to the community on a regular basis about our progress, and
- Holding a reporting and accountability session at least once yearly to report to the community on our activities and receive feedback on how we are doing.

**Community Advice and Guidance**

We commit to welcoming and considering public advice and guidance. We invite Salt Spring residents to provide this by:

- Sending us e-mails or letters,
- Speaking as a delegation at an LCC meeting,
- Inviting LCC members to attend community meetings and other gatherings.”

**12. New Business**

**12.1 Determine Next Meeting Date**

**MOVED** by Chair Rook, **SECONDED** by Commissioner Webster,  
That the Salt Spring Island Local Community Commission request staff schedule a Special meeting on Tuesday, July 11, 2023 to discuss dissolved commission projects with former Commissioners from the Salt Spring Island Parks and Recreation Commission, Salt Spring Island Community Economic Sustainability Commission, Salt Spring Island Liquid Waste Disposal Local Services Commission and the Salt Spring Island Transportation Commission and, to further discuss the 2023 Operating and Capital Plan Budget.

**CARRIED**

**12.2 Active Transportation Grant**

**MOVED** by Commissioner Baker, **SECONDED** by Commissioner Corno,  
That the Salt Spring Island Local Community Commission request staff add an  
Active Transportation Grant discussion to the Tuesday, July 18, 2023 Regular  
meeting agenda.

**CARRIED**

**13. Adjournment**

**MOVED** by Chair Rook, that the meeting adjourn at 1:46pm.

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**CHAIR**

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**SENIOR MANAGER**

**REGISTRATION FORM FOR DELEGATIONS TO ADDRESS  
THE CAPITAL REGIONAL DISTRICT BOARD OR A CRD COMMITTEE**

- The Board (Committee) may, by majority vote, allow a delegation to address the meeting on the **subject of an agenda item**.
- Each delegation is required to complete this form and submit it to CRD Legislative & Information Services by 4:30 pm not less than two calendar days prior to the meeting. For a Wednesday meeting, this means that the form must be received on Monday of the week of the meeting.
- If you miss this deadline, you may still submit this form; however, such requests will require unanimous approval.
- Each address shall be limited to four minutes unless a longer period is agreed to by unanimous vote.
- Any PowerPoint or video presentation which accompanies a delegation's speech must be provided to Legislative Services in its electronic form at least 24 hours in advance of the meeting.
- Rules governing delegations are outlined in Section 13 of the Capital Regional District Board Procedures Bylaw 3828.
- All open Board and Standing Committee meetings that take place in the 6<sup>th</sup> floor Board Room, 625 Fisgard Street, Victoria, will be webstreamed.

Submit form to CRD Legislative Services - Fax: (250) 360-3130

I wish to address the: ☒ CRD BOARD  
☒ LCC COMMITTEE

AT THE MEETING OF July 18<sup>th</sup>, 2023 at 9 AM/PM  
ON AGENDA ITEM Funding for SSI Abattoir

NAME Abey Scaglione

ADDRESS (optional) or AREA/MUNICIPALITY YOU RESIDE IN:

[REDACTED]

I REPRESENT SSI Abattoir / Ruckle Heritage Farm

(Name of Organization if applicable)

AS \_\_\_\_\_  
(Capacity/Office)

TELEPHONE [REDACTED] FAX \_\_\_\_\_

E-MAIL [REDACTED]

My reason(s) for appearing is (are) and the substance of my presentation is as follows:

Discussing the importance of local abattoirs for food security and the need for local processing in order to keep local animal agriculture

(If more space is required, please attach an additional page to this form.)

- ☐ I will have a PowerPoint or video presentation and will submit it at least 24 hours in advance of the meeting.
- ☒ I am aware that the meeting and my presentation will be webstreamed live via the CRD website and recorded.

\*Please note personal information contained on this form is collected under the authority of the Local Government Act and is subject to the Freedom of Information and Protection of Privacy Act. The personal information will be used for contact purposes only. Enquiries about the use of information in this form can be directed to the Freedom of Information and Protection of Privacy contact: Manager, Information Services at 250-360-3639.

July 7, 2023  
Date

Capital Regional District, P.O. Box 1000, 625 Fisgard Street, Victoria, BC, V8W 2S6  
Phone: (250) 360-3127 Fax: (250) 360-3130



Making a difference...together

RECEIVED

JUL 14 2023

## RULES GOVERNING THE HEARING OF INDIVIDUALS OR DELEGATIONS BY THE CAPITAL REGIONAL DISTRICT COMMISSIONS

- Under the CRD Procedural Bylaw, the Commission may, by *resolution*; allow an individual or a delegation to address the meeting on the subject of an agenda item, provided written application has been received by the Salt Spring Island Administration Office no later than 4:30pm two (2) calendar days prior to the meeting.
- If you miss this deadline, you may still submit this form; however such requests will require *unanimous* approval of the Commissions at the intended meeting.
- Each address should be limited to five (5) minutes unless a longer period is agreed to by unanimous vote.
- Each delegation should provide the number of copies of their written submission, as determined by the Salt Spring Administration Staff.

Submit form to Salt Spring Island Administration

E-mail: [saltspring@crd.bc.ca](mailto:saltspring@crd.bc.ca)

Fax: (250) 537-4456

Capital Regional District, 108 121 McPhillips Ave, Salt Spring Island, BC V8K 1K3

I wish to address the:

☒ LCC

☐ Parks and Recreation Commission

☐ Transportation Commission

☐ Community Economic Development Commission

AT THE MEETING OF June 18, 2023 at 9:00 AM/PM  
ON AGENDA ITEM 7.13 23-518

NAME Robin Williams

ADDRESS c/o Ganges Fire Hall

I REPRESENT Salt Spring Fire Protection District  
(Name of Organization if applicable)

AS Elected Trustee

(Title/Position)

TELEPHONE [REDACTED]

FAX [REDACTED]

E-MAIL [REDACTED]

My reason(s) for appearing is (are) and the substance of my presentation is as follows:

Supplemental information supporting the fire district request for a permanent seat on the GHSW PSC.

(If more space is required, please attach a

July 19 / 2023  
Date

[REDACTED]  
Signature



**REPORT TO LOCAL COMMUNITY COMMISSION  
MEETING OF TUESDAY, JULY 18, 2023**

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**SUBJECT**     **Salt Spring Island Active Transportation Plan – Ganges Village**

**ISSUE SUMMARY**

To present the Salt Spring Island Active Transportation Plan (SSI AT Plan) – Ganges Village for information.

**BACKGROUND**

With matching funding support from the BC Active Transportation Network Planning Grant, Watt Consulting was contracted to deliver the SSI AT Plan that focused on Ganges Village. Many grant applications require that projects submitted for funding be identified in Network Plans. Projects identified in this plan will meet this criterion.

The Pedestrian and Cycling Master Plan (PCMP) – SSI edition 2013 provides an overview and vision for the future of cycling on SSI. It is very heavily focused on cycling connectivity across the island with limited attention paid to pedestrian needs. The SSI AT plan addresses this lack of focus on pedestrian needs and takes a more integrated approach to providing a variety of transportation options for visitors and residents alike. Similarly, the PCMP SSI edition paid limited attention to the vital role that Ganges Village plays as the key service, social, employment and community center of the island. With Islands Trust working to develop a Ganges Village Area Plan, it became evident that a transportation focus on Ganges was needed. The findings from this work will inform the work of Islands Trust.

Other active transportation plans have been developed over the years on SSI. For example, the North Ganges Transportation Plan was drafted in 2014. It has served the community well and has led to important infrastructure upgrades. However, it applied to a very limited geographic area on the edge of Ganges Village.

The Ministry of Transportation and Infrastructure (MoTI) ran a parallel process to the SSI AT Plan, delivering the Salt Spring Island Cycling Safety Strategy. This plan focused on cycling safety between the island's ferry terminals. Several shared transportation priorities were identified in Ganges Village.

The SSI AT Plan looks to fill the gaps that were not addressed in the aforementioned plans, predominantly through a focus on Ganges Village and the inclusion of pedestrian and accessibility needs.

The unique nature of SSI and its available resources were at the forefront when developing the plan. The plan was designed in consultation with stakeholders to focus on quick, low cost, actionable and high impact projects and initiatives.

The plan advances the island's sustainability, safety, accessibility and livability aspirations with new infrastructure largely reallocating existing road right of way to non-vehicular users.

Implementing the priorities would be transformational for Ganges Village and the island as a whole, while setting a precedent for other island and small mainland communities.

## **ALTERNATIVES**

That the Salt Spring Island Active Transportation Plan – Ganges Village be received by the Local Community Commission for information.

## **IMPLICATIONS**

### *Alignment with Board & Corporate Priorities*

The priorities identified in the plan align with the CRD Board Transportation priority for residents to have access to convenient, green and affordable multi-modal transportation systems that enhance livability.

### *Environmental & Climate Action*

The CRD has declared a Climate Emergency. As the transportation sector generates the largest share of greenhouse gases (GHG) emissions, a focus on shifting travel from fossil fueled automobiles is essential. No and low carbon transportation options are paramount to decreasing GHG emissions on the island.

### *Equity, Diversity & Inclusion*

Providing low or no cost transportation options provides more residents with greater opportunity to participate in the community. Accessibility considerations have been considered when identifying priorities. The build out of pedestrian infrastructure would provide greater access for people with mobility and vision issues.

### *Financial Implications*

The SSI AT Plan includes a list of recommended priorities and associated high level cost estimates. There is no dedicated funding for implementing the priority projects from the Plan. Funding to deliver these priorities can be sourced through a variety of means such as grants, requisitions and partnerships. Decisions on how and when to fund these projects need to be balanced against other workflows and priorities.

### *Intergovernmental Implications*

CRD staff have worked closely with MoTI staff in developing this Plan and have sought feedback on each of the priority options. Islands Trust staff were engaged throughout the process and attended stakeholder engagement meetings. Partnerships between the CRD, MoTI, Islands Trust and local non-profit agencies will be critical to achieving the goals and objectives of the SSI AT Plan.

### *Growth Implications*

Representatives from Islands Trust were engaged as part of the development of this Plan. It can be used as a resource when considering future land use decisions. The engagement process made it clear that there was a strong desire to see pedestrian and cycling infrastructure improvements in Ganges.



*Service Delivery Implications*

Any decision to advance planning, design and implementation of the priority projects would need to balance the existing and planned workplans of staff.

*Social Implications*

The Plan identifies a street network that will better meet the needs of cyclists and pedestrians of All Ages and Abilities. Providing safe and accessible transportation options allows more people to access services and participate more fully in community life.

**NEXT STEPS**

Receipt of the SSI AT Plan identifying active transportation priorities for Ganges Village is the first step towards making the village more welcoming to all users, regardless of their age or ability. Most grants necessitate submissions to be shovel ready which requires design work to have been completed prior to applying. Funding design work for the priority projects and incorporating this work into staff workplans is vital if these projects are to advance from the planning stage to implementation.

**CONCLUSION**

The completion of the Salt Spring Island Active Transportation Plan provides the policy and infrastructure actions that are needed to advance, promote and deliver active transportation improvements that support the needs of residents and visitors living in, moving through and visiting Ganges Village. The Plan builds upon previous transportation plans and addresses significant gaps such as improving pedestrian travel options and general accessibility in Ganges. Transportation projects and initiatives identified advance the island's sustainability, safety and livability aspirations. The intent of the SSI AT Plan is to provide decision makers with the tools necessary to transform Ganges in to a welcoming, safe, sustainable and accessible Village center.

**RECOMMENDATION**

That the Salt Spring Island Active Transportation Plan – Ganges Village be received by the Local Community Commission for information.

Submitted by:	John Hicks, RPP, MCIP, Senior Transportation Planner, Regional and Strategic Planning
Concurrence	Karla Campbell, MBA, BPA, Senior Manager, SSI Electoral Area

**ATTACHMENT**

Appendix A: Salt Spring Island Active Transportation Plan



# Salt Spring Island Active Transportation Network Plan

July 2023





# Acknowledgements

## Land Acknowledgement

The Capital Regional District respectfully acknowledges that we live and work within the ancestral and unceded traditional territory of the Hul'qumi'num and SENĆOŦEN speaking peoples.

## Grant Acknowledgement

Funding for the Active Transportation Network Plan has been made possible through the BC Active Transportation Infrastructure Grants Program from the Ministry of Transportation and Infrastructure.

## Project Acknowledgements

WATT Consulting Group and Ahne Studio would like to thank the residents and visitors of Salt Spring Island, CRD staff, the Salt Spring Island Transportation Commission, and other stakeholders who provided invaluable feedback throughout the plan process.

The Active Transportation Network Plan was developed in a collaborative fashion, involving members of WATT Consulting Group, Ahne Studio, and the CRD. The following team members contributed to this project.

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# Executive Summary

## Overview

In 2022, the Capital Regional District (CRD) obtained funding from the BC Active Transportation Grants Program to prepare an Active Transportation Network Plan (ATNP) for Salt Spring Island (SSI). The ATNP, which builds on Salt Spring's 2013 Pedestrian and Cycling Master Plan, is intended to be a practical, cost-effective, and technically rigorous planning document that will guide active transportation planning in Ganges Village over the next 10 years. Broadly speaking, the purpose of the ATNP is three-fold:

1. Understand and document the existing challenges and barriers to active transportation within Ganges Village;
2. Provide a list of high-priority active transportation infrastructure projects for Ganges Villages that can improve quality of life, safety, and mobility for residents and visitors; and
3. Identify the policy and programmatic actions that are needed to promote active transportation and support emerging mobility options that respond to the needs of island residents.


WATT Consulting Group, in partnership with Ahne Studio and the Capital Regional District, undertook a 10-month planning process to produce the ATNP. The plan included a public and stakeholder engagement process, a detailed technical analysis, and engineering considerations to ensure that the proposed recommendations are actionable and achievable. The ATNP included two key deliverables that set the foundation for the recommendations:

1. **Baseline Conditions Report (November 2022):** the report provided a detailed analysis of Salt Spring Island's current transportation network. This included an overview of existing transportation mode share and travel patterns; a summary of the road network; a summary of the main barriers to walking, rolling, and cycling; and an outline of the key opportunities to be further explored in the ATNP through public engagement and additional technical analysis.
2. **Phase 2 Engagement Summary (November 2022):** the report summarized the initial engagement activities, where the community was asked to share their feedback on the barriers, issues, and opportunities to improve active transportation on the island. The engagement findings largely validated the technical analysis completed in the Baseline Conditions Report and allowed the project team to understand the overall priorities.

## A Focus on Ganges Village

While the ATNP was being prepared over the course of 2022, the Ministry of Transportation and Infrastructure was simultaneously undertaking the Salt Spring Island Cycling Safety Review. The purpose of that study was to explore the improvements needed for cycling safety on the major roadways that connect the ferry terminals on Salt Spring including Fulford-Ganges Road, Lower Ganges Road, and Vesuvius Bay Road. The study identifies several safety issues on the three major corridors and includes mitigation measures that span addressing speeding concerns, the provision of new cycling facilities, and intersection designs.

Based on the scope of the Salt Spring Island Cycling Safety Review, and to avoid duplicating any efforts, the Active Transportation Network Plan is focused on Ganges Village and does not provide recommendations



for improvements across the island. Further, the focus on Ganges Village is consistent with the original scope of work requested by the CRD recognizing that the 2013 plan did not provide detailed direction on the improvements needed for the village.

## Vision & Guiding Principles

A vision statement was created for the Active Transportation Network Plan to capture what Ganges Village aspires to be in 10 years from now. The vision statement was informed by the public engagement undertaken as part of the ATNP and reflects the broader transportation themes from the Ganges Village Local Area Plan process.

**Ganges Village is a place to gather, where the community—including residents, visitors, and those employed in the local economy—can move around the village seamlessly, safely, and actively by any mode of active transportation. The abundance of communal gathering spaces reflects the vibrancy and livability of Ganges Village. The active transportation network has been transformed, where a larger share of the roads has been reallocated to people walking, rolling, and cycling. All destinations within the village are easily accessible for all users, especially those with mobility challenges. Ganges Village is connected to the larger island, accessible, and safe for all where active transportation is the preferred way to move around—lowering the community’s GHG emissions, contributing to the local economy, and allowing Ganges to realize its full potential as a complete community and as a cultural tourism destination.**

Five guiding principles were developed for the Active Transportation Network Plan. They reflect the values of active transportation planning on Salt Spring and serve as the framework for the plan’s recommendations.

- 1. Low-cost, Quick-build, & Actionable Solutions:** A quick-build approach is always explored first for all active transportation improvements to minimize costs and build new projects in a time efficient manner.
- 2. A Pedestrian Friendly Village:** Ganges Village is a pedestrian friendly village that maximizes space for people. The Village is designed in a way that maximizes safety and comfort for those walking and rolling while retaining on-street parking where possible.
- 3. Integration & Connectivity:** Ganges Village is part of the island’s larger active transportation network that enables safe and convenient connections to key community destinations.
- 4. A More Accessible Ganges:** An active transportation environment that enables residents and visitors of all abilities to walk and roll safely, especially those with mobility aids or strollers.
- 5. Active Transportation Solutions are Climate Solutions:** Consistent with the CRD’s Regional Climate Action Strategy, new active transportation infrastructure can mitigate climate impacts where on-road transportation represents the largest source of the region’s greenhouse gas (GHG) emissions. All active transportation solutions will be positioned, framed, and articulated in the larger context of meeting the CRD’s commitment to reduce GHG emissions by 40% by 2030.



## Strategic Framework

### Strategy Area 1 Reallocate Street Space for Walking, Rolling, and Cycling

#### Action 1A:

Implement McPhillips Avenue pedestrian facilities

#### Action 1B:

Implement Rainbow Road pedestrian facilities

#### Action 1C:

Implement Fulford Ganges Road pedestrian and cycling facilities

#### Action 1D:

Implement Lower Ganges Road pedestrian and cycling facilities

#### Action 1E:

Adopt infrastructure design guidelines

#### Action 1F:

Connect to active transportation network outside of Ganges Village

### Strategy Area 2 Make Ganges Village Accessible for all Residents and Visitors

#### Action 2A:

Conduct a land ownership survey of the Ganges Village

#### Action 2B:

Adopt a 30 km/h posted speed limit in Ganges Village

#### Action 2C:

Create village gateways with traffic calming design interventions

#### Action 2D:

Undertake reviews of intersections within Ganges Village

#### Action 2E:

Retain accessible parking within the Village

#### Action 2F:

Establish no-parking zones with physical barriers where necessary

### Strategy Area 3 Encourage Sustainable Modes

#### Action 3A:

Develop a bike parking retrofit program

#### Action 3B:

Identify opportunities to increase off-street parking

#### Action 3C:

Develop a parking wayfinding program

#### Action 3D:

Work with BC Transit to integrate active transportation modes

Big Move

## The Five Big Moves

A total of 26 actions are recommended in the ATNP. The implementation of each action will require time, resources, and financial commitments. The recommended sequencing of the actions is shown in Section 8.3 (Action Plan). While all the actions in the ATNP are critical for helping meet the plan's vision, some are deemed to be of higher importance to have a more immediate and direct impact on Ganges' active transportation network.

The five big moves were selected based on meeting at least one of the following criteria:

- 1. Low-cost.** In alignment with the first guiding principle of the ATNP, a big move should be relatively low cost recognizing the limited financial resources for infrastructure on the island.
- 2. Actionable.** In alignment with the first guiding principle of the ATNP, a big move should be actionable, meaning that it requires minimal staff time and strong support from key stakeholders such as MOTI to make implementation seamless.
- 3. High Impact.** High impact refers to increasing the number of people walking, cycling, and rolling while improving safety and accessibility for the most vulnerable road users.

The “big moves” are identified in the strategic framework with their corresponding action number. They have not been listed based on their order of priority; ultimately, the CRD and its partners will determine what is most feasible to implement first based on funding opportunities, staffing, and overall resources.

## Implementation Strategy

The five “big moves” are recommended to be implemented first. The overall cost for big moves 1, 4, and 5, is anticipated to be relatively low and will depend on several factors such as overall signage requirements, the complexity of the land ownership survey, and whether the CRD acquires land for a new off-street parking location. Based on the Class D cost estimates, the total cost of Big Move 2 (Action 1A) and Big Move 5 (Action 1C) is approximately \$432,000.

The total level of investment for all of the proposed active transportation corridor improvements is approximately \$1,647,000 excluding GST. The implementation strategy includes an overall action plan for each of the recommendations in the ATNP and the different funding opportunities available to pay for the facility improvements.

## Next Steps

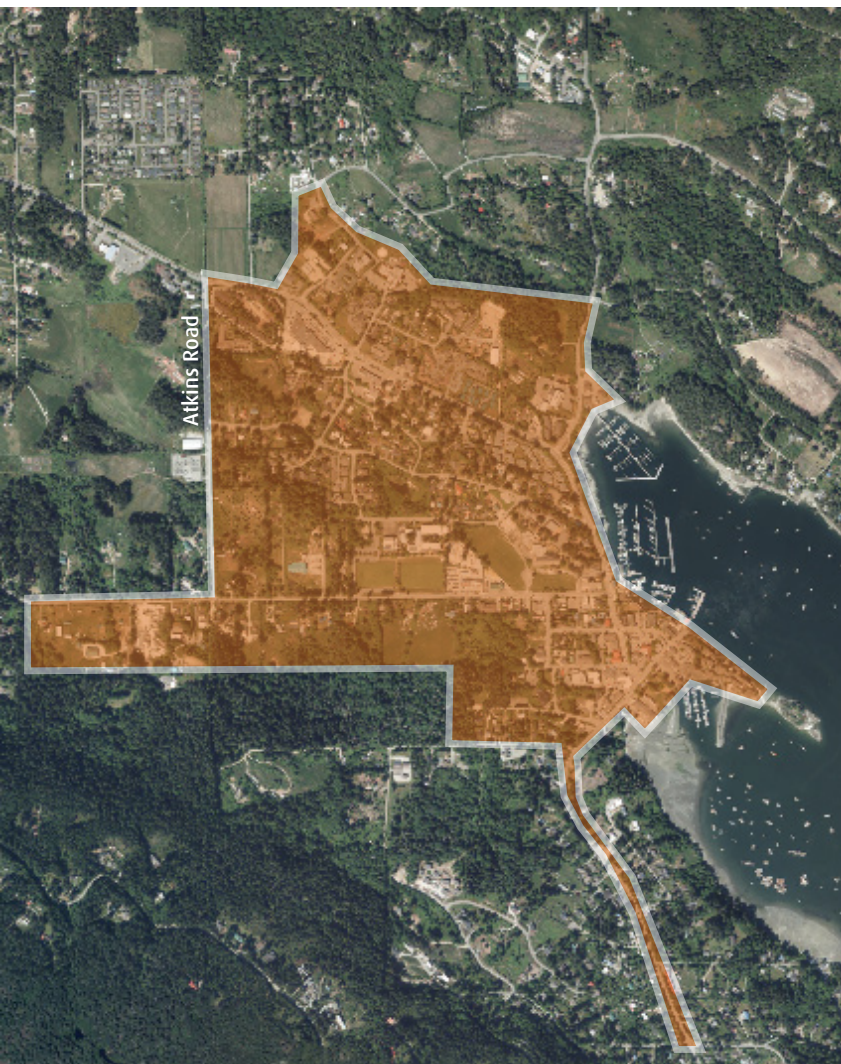
The ATNP is intended to move the community on a path toward better, safer, and more inclusive active transportation. As a smaller community with limited financial and staff resources, the CRD, and its associated partners, will need to move quickly on the low-cost, quick-build solutions identified in the ATNP. Further, the ATNP should be a living document and not sit on the proverbial bookshelf collecting dust. It is recommended that the plan be revisited in 5 years (2028) to explore what has—and has not—been implemented to date and whether any of the actions in this plan need to be reprioritized. A more walkable, bike friendly, and accessible Ganges Village will take time, and is anticipated to boost the local economy, lower the community's GHG emissions, and enhance safety for all.

# 1.0 Introduction

## 1.1 Purpose

In 2022, the Capital Regional District (CRD) obtained funding from the BC Active Transportation Grants Program to prepare an Active Transportation Network Plan (ATNP) for Salt Spring Island (SSI). The ATNP, which builds on Salt Spring's 2013 Pedestrian and Cycling Master Plan, is intended to be a practical, cost-effective, and technically rigorous planning document that will guide active transportation planning on the island over the next 10 years. Broadly speaking, the purpose of the ATNP is three-fold:

1. Understand and document the existing challenges and barriers to active transportation within Ganges Village;
2. Provide a list of high-priority active transportation infrastructure projects for Ganges Villages that can improve quality of life, safety, and mobility for residents and visitors; and
3. Identify the policy and programmatic actions that are needed to promote active transportation and support emerging mobility options that respond to the needs of island residents.



### Ganges Focus

The Active Transportation Network Plan is focused on Ganges Village and does not provide recommendations for improvements across the island. The focus on Ganges Village is consistent with the original scope of work requested by the CRD, recognizing that the 2013 plan did not provide detailed direction on the improvements needed for the village. Further, while the ATNP was being prepared over the course of 2022, the Ministry and of Transportation and Infrastructure was undertaking the Salt Spring Island Cycling Safety Review. The study's purpose was to explore the improvements needed for cycling safety on the major roadways that connect the ferry terminals on Salt Spring including Fulford-Ganges Road, Lower Ganges Road, and Vesuvius Bay Road. The study identifies several safety issues on the three major corridors and includes mitigation measures that span addressing speeding concerns, the provision of new cycling facilities, and intersection designs.

To avoid duplicating the analysis and recommendations found within the Salt Spring Island Cycling Safety Review, the Salt Spring Island ATNP does not include any recommendations on the three major corridors but does address the need to enhance connectivity from Ganges Village to those corridors.

## 1.2 Plan Process



Phase 1  
Understanding

The Salt Spring Active Transportation Network Plan launched in July of 2022 was led by WATT Consulting Group in collaboration with Ahne Studio, the Capital Regional District, key stakeholders, and community members. Through a highly collaborative planning process, the voices of community members and transportation experts were captured to help shape the future of active transportation with Ganges Village. The plan was created in four distinct phases, which are detailed below:



Phase 2  
Listening

The first phase, **Understanding**, culminated with the Salt Spring Island ATNP Baseline Conditions Report that included a detailed analysis of the barriers and opportunities around active transportation within Ganges Village and across the island.



Phase 3  
Developing

The second phase, **Listening**, included the initial engagement work, where the community was asked to share their feedback on the barriers, issues, and opportunities to improve active transportation on the island. The community feedback was summarized in the Salt Spring Island ATNP Phase 2 Engagement Summary.



Phase 4  
Developing

The third phase, **Developing**, included a detailed assessment of the potential active transportation corridors in Ganges; producing the draft vision and guiding principles; and establishing the key recommendations and “big moves” for the ATNP. The Developing phase also included additional engagement with the community where a stakeholder workshop and online survey were undertaken to collect feedback on the draft ATNP vision, guiding principles, and preliminary recommendations.

The fourth and final phase, **Finalizing**, included finalization of the ATNP and a presentation to the Local Community Commission.

## 1.3 What is Active Transportation?

The most comprehensive definition of active transportation in the BC context is provided in the BC Active Transportation Design Guide (BC ATDG). It is defined as follows:

“Any form of human-powered transportation, including walking, cycling, or rolling using a skateboard, in-line skates, wheelchair, or other wheel-based forms of human-powered transportation. It also includes winter-based active modes, water-based active modes, and horseback riding, although these modes are typically more recreational in nature.” Active transportation can also include emerging mobility (or new mobility) travel modes made possible by advances in technology including electric bicycles and electric scooters.

According to the guide, active transportation should be based on inclusive mobility to ensure that the network is comfortable, convenient, safe, and attractive for everyone, regardless of age or ability. A full definition of active transportation, including its benefits and key principles of inclusive mobility, is provided in Chapter B of the BC ATDG.



## 2.0 Community Context

### 2.1 Location

Located on the ancestral and unceded traditional territories of the Hul'q'umi'num and SENĆOTEN speaking peoples, Salt Spring Island is the largest and most populous of the Southern Gulf Islands within the Capital Regional District. Salt Spring is serviced by BC Ferries along three routes linking Fulford Harbour with Swartz Bay, Vesuvius to Crofton, and Long Harbour to Tsawwassen mainland.

### 2.2 Demographic Highlights

As of 2021, Salt Spring was home to 11,635 year-round residents with the population frequently doubling in the summer with an influx of visitors. The island has experienced rapid population growth in the last five years as compared to the previous census period. Additionally, the percentage of those in the 60 years and older age group has increased over the last five years (from 31% to 35%). An aging population has implications for the active transportation network as some people may not be able to drive and may desire shorter walking trips to access their destination.

#### By the numbers...

**11,635**  
year-round residents

**10.2%**  
growth in the number of  
Salt Spring Island residents  
between 2016 and 2021

**56.4**  
is the median age for the  
Island

**35%**  
of the population is 65  
years old+

**12%**  
of the population is  
between 0-14 years old





Gabriola  
Island

Metro  
Vancouver

Valdes  
Island

Thetis  
Island

Tsawwassen

Penelakut  
Island

Galiano  
Island

Mayne  
Island

Saturna  
Island

Pender Island

Vancouver  
Island

Salt Spring  
Island

Prevost  
Island

Vesuvius

Long Harbour

Fulford Harbour

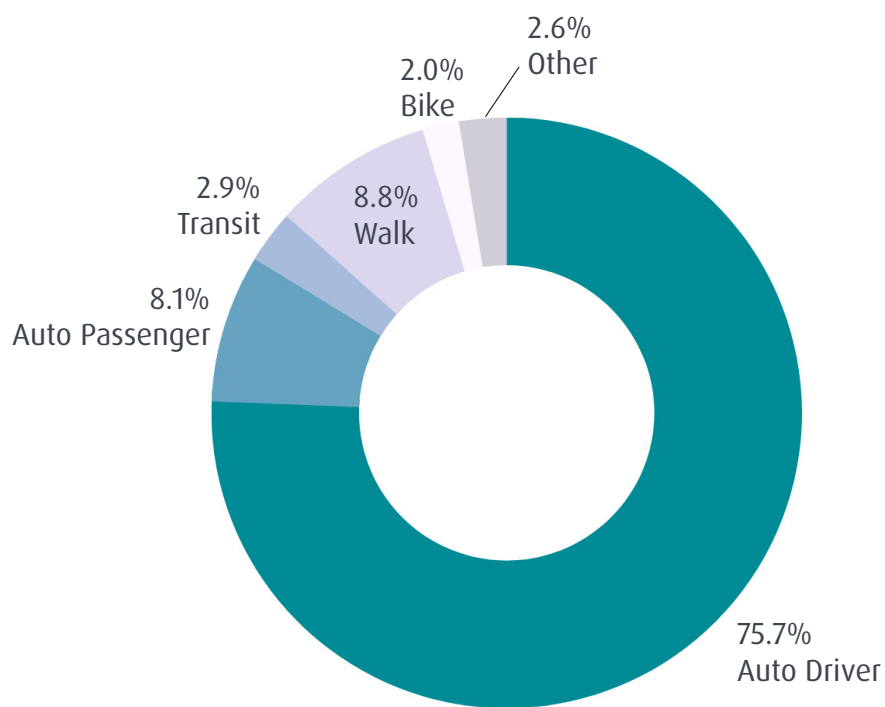
Swartz Bay

Crofton

## 2.3 How Salt Spring Islanders Get Around Today

As of 2021, 84% of commuting trips by Salt Spring Island residents were completed as an auto driver or passenger. Walking makes up most of the sustainable commuting mode share (transit, walk, or bike) on the island at 8.8%.

Going beyond commuting trips, the CRD's 2017 Origin Destination Household Travel Survey provides greater detail into the overall travel patterns on the island. According to the survey, there are approximately 25,000 trips on a typical day on Salt Spring. About 70% of these trips are by auto driver with auto passenger trips comprising 18% of the total mode share, which is likely attributed to the aging population and their dependency on others to drive. Walking makes up most of the sustainable mode share within the district at 6% with cycling and transit accounting for only 2% and 1%, respectively.



Statistics Canada 2021 Journey to Work, Salt Spring Island







## By the numbers...

**1.69**

vehicles per household  
(2017)

**5.15**

daily trips per household  
(2017)

**84%**

driving mode share  
(2021)

**11%**

active transportation  
mode share  
(2021)

**61%**

of all commutes are less  
than 15 minutes in duration  
(2021)

**91%**

of all commutes stay with-  
in Salt Spring Island  
(2021)



## 2.4 Challenges Unique to Salt Spring

As a Gulf Island, Salt Spring has unique challenges that have implications for active transportation planning and that make its community context different from other jurisdictions in the province.

### Complex Jurisdictional Framework

Salt Spring Island has a complex jurisdictional framework. As an unincorporated rural Electoral District, it falls under the jurisdiction of the Capital Regional District, which functions as the local government and handles the administrative functions. As an unincorporated area, all roadways on Salt Spring Island are under the jurisdiction of the Ministry of Transportation and Infrastructure (MoTI).

In 1974, the province passed the Islands Trust Act, which established the Islands Trust. As part of the Islands Trust Area, planning for Salt Spring Island must consider and address the unique mandate of the Islands Trust “to preserve and protect the trust area and its unique amenities and environment for the benefit of the residents of the trust area and of British Columbia generally, in cooperation with municipalities, regional districts, improvement districts, other persons and organizations and the government of British Columbia”. The Salt Spring Island Local Trust Area is one of 72 local trust areas and one Island Municipality within the Islands Trust Area. Each Local Trust Committee has been provided with most of the land use planning authority of a regional district board with a mandate, embodied in the “object” of the Islands Trust.

This complex jurisdictional and governance framework can result in more challenges with transportation planning and implementation of new transportation infrastructure.

### Seasonal Fluctuations and Ferry Traffic

Salt Spring Island is a popular tourist destination and experiences its peak tourist season between June and September when the island’s population nearly doubles. During this time, many visitors attend the community’s popular Saturday Market in Ganges Village, which features local artisans and food vendors, as well as music and other entertainment. This results in parking challenges within the village and congestion on the island’s rural road network.

Salt Spring Island is primarily accessed by ferry, and communities that rely on ferry transportation to access the mainland face a unique set of transportation challenges. Ferry transportation, particularly during peak travel times, such as holidays or weekends, results in high-volume vehicle traffic arriving at the same time. These spikes in ferry traffic volumes create less safe and comfortable conditions for active transportation users due to the limited road space for people walking and cycling near the ferry terminals.



## Existing Active Transportation Facilities Not Meeting AAA Standards

Salt Spring faces challenges in accommodating vehicle traffic on its narrow and at times steep and winding roads while retrofitting the existing right-of-way for active travellers. Most of the roads were built to a ministry standard, which means they are intended to move vehicle traffic efficiently and not intended to serve the needs of those using active transportation.

Outside of Ganges Village, the island is largely rural, hilly, scenic, and with destinations scattered across it. Walkable shoulders with no buffer and or narrow unpaved pathways with minimal protection from vehicle traffic may not feel safe or comfortable for some. The shoulders are also narrow, which often means people walking or biking have to maneuver into the vehicle travel lane in order to pass safely. Further, most of the shoulders have poor pavement conditions such as cracks, potholes, and loose gravel. The existing walking conditions within Ganges Village also vary and there are accessibility challenges for some people walking and rolling. In some cases, this includes insufficient sidewalk widths or curb letdowns, sidewalk obstructions, and unclear crossings that may not be accessible to all or leave some people feeling uncomfortable.

The CRD recently approved a new standard for all ages and abilities facilities, which generally speaks to the need for roads to be more inclusive for all users including those with mobility aids and/or children wanting to walk or roll to school, for example.





## 2.5 Policy Context

Salt Spring Island has several planning, policy, and technical studies with direct relevance to active transportation that will help shape the direction of the Active Transportation Network Plan.

### Official Community Plan (2008)

The Salt Spring Island Official Community Plan (OCP) was adopted on October 8, 2008, providing a long-term vision for the community and guiding principles on future planning, growth, and development. The OCP identifies the following priorities that have relevance to the ATNP:

- Multi-family dwellings that offer safe walking, cycling and transit links to schools, village services and one's that are near villages will be given preference.
- Support for future settlement patterns that reduce automobile dependency and encourage walking, cycling and public transit.
- Village containment boundary policies for Ganges, Fulford and Channel Ridge to keep the development compact.
- Support for multi-modal transportation for inter-village travel.

SSI's OCP recognizes the desire to limit automobile dependence and provides policy direction to integrate walking and cycling within the public transit system.

### Pedestrian and Cycling Master Plan (2013)

The Pedestrian and Cycling Master Plan (PCMP): Salt Spring Island Edition recognizes the area's unique character and transportation context. It also outlines the current and desired level of development consistent with the vision used in developing the network for the Regional PCMP and previous bicycle planning efforts on the island. This plan acknowledges transit's integral role in ensuring the success of an active transportation strategy and includes considerations such as:

- Appropriately planning for expected demand
- Providing connections between active transportation and transit networks
- Providing appropriate facilities at transit destinations and stops
- Creating convenient pedestrian and cycle access to transit destinations and stop
- Accommodating a wide range of pedestrians and cyclists in the design of transit destinations and stops

The plan considers the needs of pedestrians and cyclists through the concept of universal design. Its goal is to provide familiar and safe environment for both in urban and rural communities across the region.

### Ganges Village Area Plan (Ongoing)

The SSI Local Trust Committee adopted resolutions approving the Project Charter to launch the Ganges Village Area Plan planning process on November 10, 2020. The plan focuses on:

- Supporting the community's social, environmental, economic, and cultural goals
- Tackling the causes and impacts of climate change (net-zero emissions, promoting and integrating sustainable mode share)
- Ensuring that development occurs in a sustainable, coordinated, and resilient manner addressing the issues impacting the social, cultural, and environmental sustainability of Ganges Village

Some of the engagement feedback heard so far through the Ganges Village Area Plan process include:

- The poor condition of sidewalks and crosswalks
- Accessibility concerns, noting that walking is especially hazardous for older people and those with mobility challenges
- The car-centric nature of the village makes it noisy and less attractive/enjoyable

There is strong desire for a more walkable village that could include pedestrian only areas; traffic calming; wider and more accessible sidewalks; and cycling facilities.

### **Salt Spring Elementary Active School Travel Report (2020/21)**

Salt Spring Elementary School participated in a school catchment / neighbourhood-based planning process with the aim to increase rates of students walking and rolling to/from school. Located in Ganges Village, the school's catchment area also extends to serve students living in Long Harbour, Fulford and Ganges. Key findings of the baseline survey and report include:

- 30% of students used active travel to get to and from school or the school bus stop
- 70% of families chose to travel by car or carpool to and from school or the bus stop
- Almost 10% of students were driven to and from school but live within 1.5km of Salt Spring Elementary
- Of all the Salt Spring Elementary families who usually drive, 82% of families would prefer walk & wheel, but choose to drive because of traffic safety concerns & convenience/time pressure

The Salt Spring Active School Travel Report highlights the unrealized potential of converting vehicle trips related to school arrival and dismissal. The report identifies several tips and strategies for families to encourage them to walk and bike to school. However, what is needed is new active transportation infrastructure that is safe, comfortable, and connected to increase the share of walking and cycling to/from school.

### **Salt Spring Island Transit Future Service Plan (2021)**

The Salt Spring Island Transit System has seen ridership grow from 100,000 riders in 2014/15 to 116,000 riders in 2019/20. The key objectives for Salt Spring Island include the electrification of their transit fleet and better integrating transit service with active modes of transportation and ferry service.

Two additional bays are needed within the existing bus stop in Ganges Village for the proposed increase of peak service. This will result in the removal of on-street parking along Lower Ganges Road in front of the Salt Spring Visitor Information Centre.

### **BC Active Transportation Design Guide (2019)**

The BC Active Transportation Design Guide (BC ATDG) was released in June 2019. The guide is a comprehensive set of planning and engineering guidelines offering recommendations for the planning, selection, design, implementation, and maintenance of active transportation facilities across the province. It contains engineering principles and best practices from the municipal, provincial, national, and international levels. These two efforts call for more protected cycling facilities generally using motor vehicle speeds and volumes as the primary way to determine what sort of facility should be provided.

As the ATNP was funded from the BC Active Transportation Infrastructure Grants Program, all of the recommended active transportation facilities in this plan must align with the BC ATDG.

## 3.0 Public & Stakeholder Engagement

The ATNP included a robust public and stakeholder engagement process to ensure that the final recommendations reflected the needs and priorities of island residents, particularly those living in proximity to Ganges Village. Over the course of the plan process, the community was invited to participate in two rounds of engagement. Each of the engagement activities were promoted using several tools, including the project webpage (Get Involved), social media platforms, and emails to stakeholders.

The engagement level – as defined by the International Association of Public Participation (IAP2) Framework’s and included in CRD’s Public Participation Framework was “involve”. This means that the project team will “work with the public to ensure concerns and aspirations are understood and considered”. The CRD commits to working “with the public to exchange information, ideas, and concerns”.

A summary of the engagement activities included in the ATNP is provided below. For a detailed summary of the engagement feedback from the first round, please see the Salt Spring Island ATNP Phase 2 Engagement Summary Report.

Across the two rounds of engagement for the ATNP process, over 500 participants shared their input...

**252**  
online survey #1  
respondents

**155+**  
ideas fair  
participants



**20**  
interactive mapping activity  
participants



**14**  
residents provided their  
input through an ASK Salt  
Spring conversation



**13**  
in-person stakeholder  
workshop participants

**82**  
online survey #2  
respondents

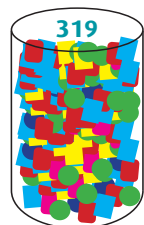




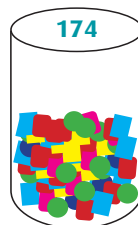


## Ideas Fair

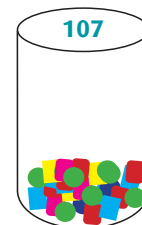
Over 150 people offered their ideas, experiences, and perspectives at an in-person Ideas Fair, identifying barriers they face in accessing Salt Spring Island's active transportation network and opportunities they would like to see explored in the plan. The event also included an activity called "planner for the day" where participants were provided with six tokens and asked to place them in an active transportation improvement category based on their top priorities. 'Cycling facilities' was the top priority (319 tokens) followed by pedestrian facilities with 174 tokens.



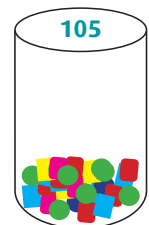
**Cycling Facilities**  
(Bike Boulevards, Bike Lanes, Multi-use Trails)



**Pedestrian Facilities**  
(Sidewalks, Pathways)



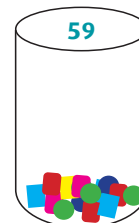
**Traffic Calming**  
(Reduced Speed Limits, Speed Humps, Traffic Circles)



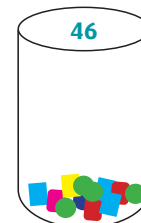
**Crosswalks to Improve Pedestrian Safety**



**Education and Programming to Promote Active Transportation**



**Secure Bicycle Parking at Key Destinations**  
(Ferry Terminals, Ganges Village)



**Wayfinding and Signage**



## Online Survey #1

To better understand the existing conditions of Salt Spring's active transportation network, an online survey was open to the public on the CRD's Get Involved platform from October 3rd to 31st, 2022. A total of 252 surveys were submitted where Salt Spring residents and visitors shared their experiences, preferences, and ideas for the future of active transportation on the island. An interactive mapping activity was also available on the CRD's Get Involved platform. Participants were asked to identify barriers that they experience as a person walking, rolling, or cycling around Salt Spring Island. 20 participants provided locations on the map, identifying opportunities for improvements, including locations for new bike lanes, better maintenance of existing paths, and unsafe intersections.

## ASK Salt Spring

On October 28th, 2022, a group of 14 residents joined CRD's Transportation Manager for an ASK Salt Spring conversation. Through this discussion, participants learned about the Active Transportation Network Plan process and shared their concerns, experiences, and ideas for moving around the island. Participants also shared their concerns and solutions to enhance pedestrian and cycling safety in the village, including changing/adding crosswalks, adding stop signs at Ganges intersections, mini-roundabouts, and reduced (and enforced) speed limits.

## Salt Spring Island Transportation Commission Presentation

On November 28, 2022, the project team presented a summary of the first two phases of the ATNP project to the Salt Spring Island Transportation Commission. This included a summary of the baseline conditions report and the first round of engagement. Committee members had an opportunity to ask questions and provide feedback on the work to date and the specific items they would like to see further explored in the latter phases of the ATNP.







## Stakeholder Workshop

An in-person stakeholder workshop was undertaken on February 7th, 2023 at Salt Spring's Public Library. The three-hour workshop provided an opportunity for a detailed discussion on the proposed active transportation improvements within Ganges Village. The project team provided a project overview of the work undertaken to date and presented the proposed corridor improvements. Facilitators led the group through a discussion around the various tradeoffs associated with the proposed improvements and gathered feedback on their level of support and preferences. Several cross-section options were presented for each corridor that were being explored in Ganges Village. Most were refined and modified based on feedback from the group while some were eliminated entirely based on consensus from the participants.



Of the 17 stakeholders invited, the following stakeholders were in attendance:

- BC Transit
- Chamber of Commerce
- Country Grocer
- Cycling Salt Spring
- Island Pathways
- Islands Trust (attended as an observer)
- Ministry of Transportation and Infrastructure
- Salt Spring Island Housing Council Society
- Salt Spring Island Transportation Commission (attended as an observer)
- Salt Spring Seniors Centre
- Salt Spring Solutions
- Salt Spring Library
- Windsor Plywood



## Online Survey #2

To better inform the final list of recommendations for the ATNP, a second online survey was open to the public on the CRD's Get Involved platform from March 3rd to 24th, 2023. A total of 82 surveys were submitted where participants provided feedback and preferences on the draft network and infrastructure improvements.



# 4.0 Plan Framework

## 4.1 Vision

A vision statement was created for the Active Transportation Network Plan to capture what Ganges Village aspires to be in 10 years from now. The vision statement was informed by the public engagement undertaken as part of the ATNP and reflects the broader transportation themes from the Ganges Village Local Area Plan process.

**Ganges Village is a place to gather, where the community—including residents, visitors, and those employed in the local economy—can move around the village seamlessly, safely, and actively by any mode of active transportation. The abundance of communal gathering spaces reflects the vibrancy and livability of Ganges Village. The active transportation network has been transformed, where a larger share of the roads has been reallocated to people walking, rolling, and cycling. All destinations within the village are easily accessible for all users, especially those with mobility challenges. Ganges Village is connected to the larger island, accessible, and safe for all where active transportation is the preferred way to move around — lowering the community’s GHG emissions, contributing to the local economy, and allowing Ganges to realize its full potential as a complete community and as a cultural tourism destination.**

## 4.2 Guiding Principles

Five guiding principles were developed for the Active Transportation Network Plan. They reflect the values of active transportation planning on Salt Spring and serve as the framework for the recommendations in this plan.

### 1 Low-cost, Quick-build, & Actionable Solutions

A quick-build approach is always explored first for all active transportation improvements to minimize costs and build new projects in a time efficient manner.

### 2 A Pedestrian Friendly Village

Ganges Village is a pedestrian friendly village that maximizes space for people. The Village is designed in a way that maximizes safety and comfort for those walking and rolling while retaining on-street parking where possible.

### 3 Integration & Connectivity

Ganges Village is part of the island's larger active transportation network that enables safe and convenient connections to key community destinations.

### 4 A More Accessible Ganges

An active transportation environment that enables residents and visitors of all abilities to walk and roll safely, especially those with mobility aids or strollers.

### 5 Active Transportation Solutions are Climate Solutions

Consistent with the CRD's Regional Climate Action Strategy, new active transportation infrastructure can mitigate climate impacts where on-road transportation represents the largest source of the region's greenhouse gas (GHG) emissions. All active transportation solutions will be positioned, framed, and articulated in the larger context of meeting the CRD's commitment to reduce GHG emissions by 40% by 2030.

#### What We Heard

In the second survey, we asked the community *"What is your level of support for draft guiding principles?"*

Over **89%** of survey respondents indicated they strongly support or support the guiding principles.

Guiding principle #5 (Active Transportation Solutions are Climate Solutions) was added based on feedback heard at the stakeholder workshop.

## 4.3 Strategy Areas

The ATNP includes three strategy areas that provide specific direction on how active transportation improvements can be realized within Ganges Village. The strategy areas build on the guiding principles above and contain recommended actions that were informed by the public engagement process and technical analysis undertaken as part of developing the ATNP.

### Strategy Area 1

#### Reallocate Street Space for Walking, Rolling, and Cycling

**Action 1A:**

Implement McPhillips Avenue pedestrian facilities

**Action 1B:**

Implement Rainbow Road pedestrian facilities

**Action 1C:**

Implement Fulford Ganges Road pedestrian and cycling facilities

**Action 1D:**

Implement Lower Ganges Road pedestrian and cycling facilities

**Action 1E:**

Adopt infrastructure design guidelines

**Action 1F:**

Connect to active transportation network outside of Ganges Village

### Strategy Area 2

#### Make Ganges Village Accessible for all Residents and Visitors

**Action 2A:**

Conduct a land ownership survey of the Ganges Village

**Action 2B:**

Adopt a 30 km/h posted speed limit in Ganges Village

**Action 2C:**

Create village gateways with traffic calming design interventions

**Action 2D:**

Undertake reviews of intersections within Ganges Village

**Action 2E:**

Retain accessible parking within the Village

**Action 2F:**

Establish no-parking zones with physical barriers where necessary

### Strategy Area 3

#### Encourage Sustainable Modes

**Action 3A:**

Develop a bike parking retrofit program

**Action 3B:**

Identify opportunities to increase off-street parking

**Action 3C:**

Develop a parking wayfinding program

**Action 3D:**

Work with BC Transit to integrate active transportation modes

Big Move

## 4.4 Key Infrastructure Facility Type Definitions

The following infrastructure types are frequently referenced throughout this document. Expanded definitions can be found in Section 5.0 'Action 1E'.

### Key Definition: Adaptive Sidewalk

Adaptive sidewalks are often used where there is no existing sidewalk or where it may be difficult to construct a raised sidewalk. They typically include a barrier from the roadway.



### Key Definition: Multi-use Pathways

Multi-use pathways (MUPs) are typically off-street pathways that are separated from motor vehicle traffic and can be used by any active transportation user.



### Key Definition: Protected Bicycle Lane

A designated lane for people cycling and other active transportation users that is physically separated from motor vehicle traffic and people walking and rolling.



### Key Definition: Painted Bicycle Lane

Separate travel lanes designated exclusively for bicycle use that are delineated by a painted line and, in some cases, a painted buffer area.



## 5.0 Strategy Area 1

# Reallocate Street Space for Walking, Rolling and Cycling

Strategy area 1 is focused on roadway reallocation to prioritize the needs of people cycling, walking, and rolling. A total of 11 actions are recommended to mitigate the gaps in the active transportation network and address the feedback heard through the public engagement process where participants identified the lack of shoulders and insufficient road space as the main barriers to walking and cycling on the island and within Ganges Village. The engagement process also confirmed that providing more separation and protection from vehicles along corridors is among the top priority for residents to improve their active transportation experience and address road safety concerns.



In addition to the public engagement process, transportation data indicate the need for greater separation of vehicles and active transportation modes as evidenced by the following:

- Currently within Ganges Village, there are limited pedestrian and cycling facilities. Sidewalks are provided on Lower Ganges Road, Rainbow Road, and other select blocks. A painted bicycle lane is also available for part of Lower Ganges Road. Except for 95 metres of separated sidewalk on Fulford-Ganges Road, none of these facilities meet the definition of all ages and abilities per the BC Active Transportation Design Guide.
- According to BC Ferries, there were a total of 15,756 bicycle arrivals at Fulford, Vesuvius Bay and Long Harbour between April 2021 to September 2021, which represents the busier tourist season. This number increased to 17,879 bicycle arrivals in 2022 for the same months, or a 13% increase. Even though the data do not indicate which roads people cycling take once they arrive on the island, many do cycle through Ganges Village via Fulford-Ganges Road and Lower Ganges Road where cycling facilities are not present and/or not protected from motor vehicles.
- According to MOTI, based on a July 2022 traffic count the motor vehicle volumes on Fulford-Ganges Road are in the range of 2,950-3,050 vehicles per day, with nearly 7 to 10% as heavy vehicles. And for some segments of the road, including within Ganges Village, the average AM vehicle speeds were 10% or more above the posted speed limit. According to the BC Active Transportation Design Guide, in rural contexts, traffic volumes above 2,500 vehicles a day justify a bicycle lane (bicycle accessible shoulder in the BC Active Transportation Design Guide).
- The Strava data indicate high cycling activity along Fulford-Ganges Road and Lower Ganges Road. Strava is a mobile app tracking human exercise and incorporates social network features. It is mostly used for cycling and running using GPS data.

Based on the engagement results and technical data, several corridor improvements are recommended and detailed in the following actions. The corridor improvements, once implemented, would result in 1.9 kilometres of new active transportation facilities. Conceptual cross-sections are included in this section to illustrate the recommended active transportation corridors.

## What We Heard

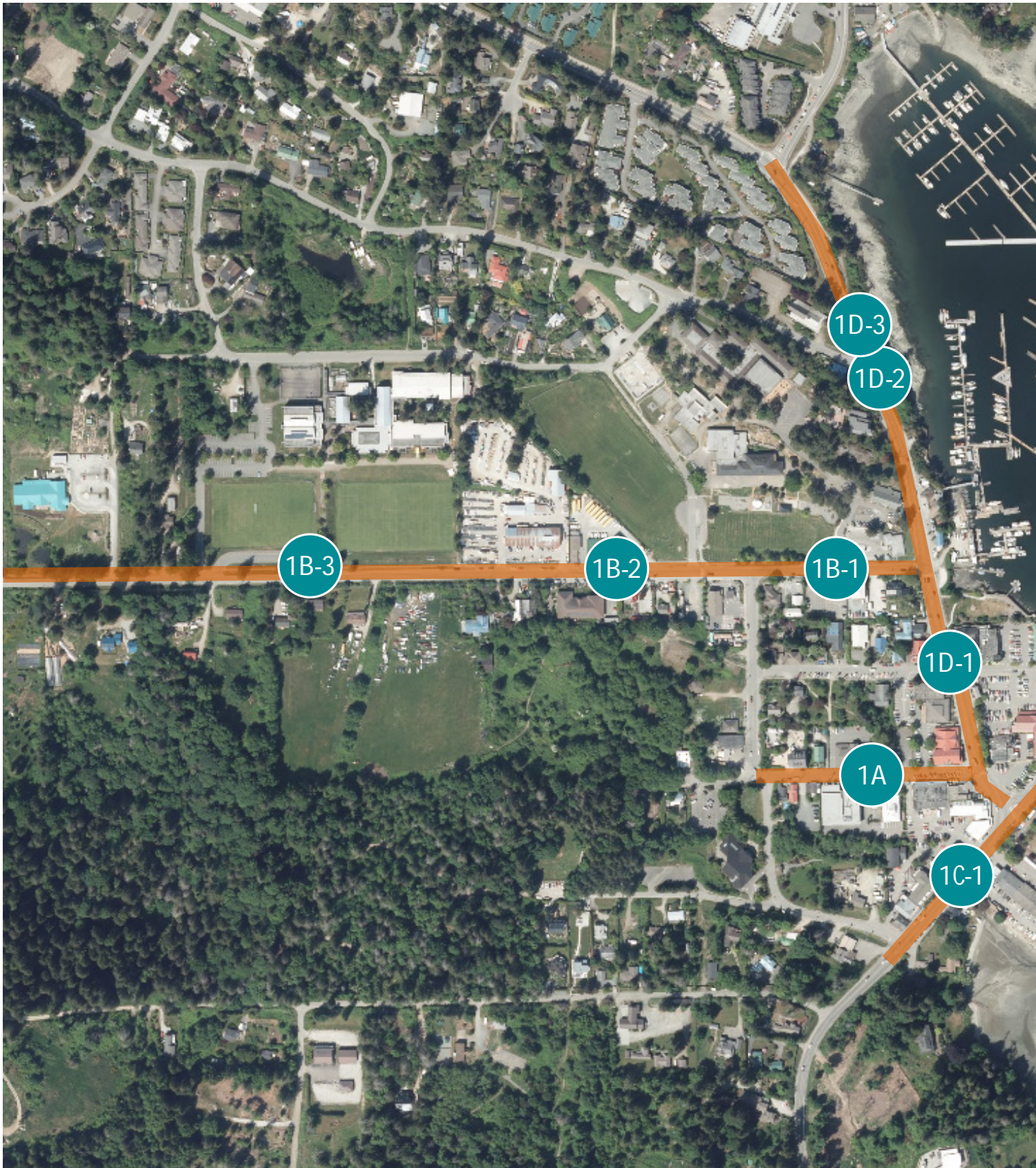
*"If there was a proper bike lane - doesn't even need to be too wide - particularly along Fulford Ganges road, I would be much more inclined to use my e-bike to ride into Ganges. As it is now, it is a treacherous experience."*

*"Vehicles, bikes and people do not share lanes well. Separation is needed."*

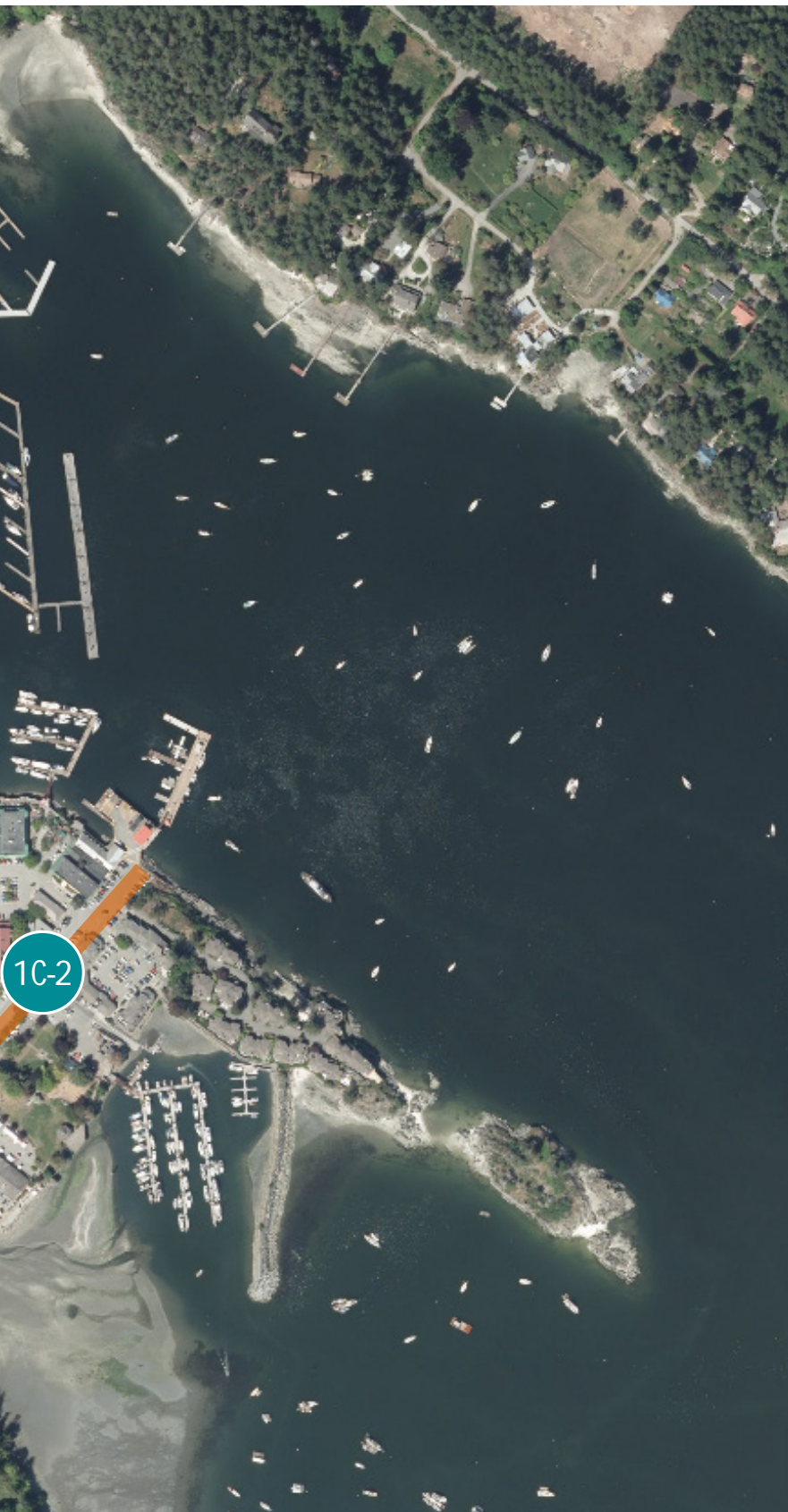
*"A walkable island that encourages active forms of transportation. Bike/walking paths throughout the island especially leading into Ganges. A layout in Ganges that makes walking the most efficient way to get around. Generally a feeling that walking and biking is safe. As it is now we hesitate to do it as it often is stressful especially with a young child. I didn't think we would move to such a beautiful place and end up driving more than we did in the city."*

*"Transport is built with cycling, pedestrians, and those with mobility issues first: and cars second. A place where an 80 year old and an 8 year old can travel safely on foot/bike, without fear."*









## Priority Corridors

### 1A McPhillips Avenue

From Jackson Ave to Lower Ganges Rd

### 1B-1 Rainbow Road

From Lower Ganges Rd to Jackson Ave

### 1B-2 Rainbow Road

From Kanaka Rd to Windsor Plywood

### 1B-3 Rainbow Road

West of Windsor Plywood

### 1C-1 Fulford Ganges Road

From Seaview Ave to Lower Ganges Rd

### 1C-2 Fulford Ganges Road

From Lower Ganges Rd to Purvis Lane

### 1D-1 Lower Ganges Road

From Fulford Ganges Rd to Rainbow Rd

### 1D-2 Lower Ganges Road

From Rainbow Rd to Upper Ganges Rd  
(Interim Solution)

### 1D-3 Lower Ganges Road

From Rainbow Rd to Upper Ganges Rd  
(Ultimate Solution)

## Action 1A Implement McPhillips Avenue Pedestrian Facilities

McPhillips Avenue is a critical east-west connection within Ganges Village. It has a sidewalk on the north side and limited pedestrian facilities on the south side. Based on the 2018 Ganges Village Parking Study, there are a total of 31 formal on-street parking spaces with different configurations including parallel, perpendicular, and angled. People also park informally along this corridor to get closer to their destinations.

The proposed cross-section is shown below and is summarized as follows:

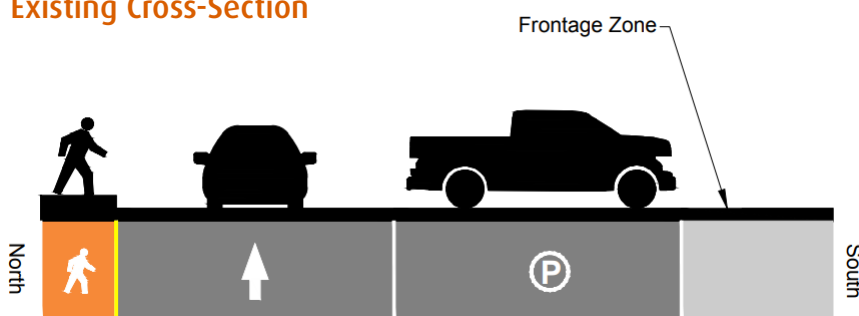
- The existing vehicle travel lane would be reduced from 5.5 metres to 4 metres.
- The existing angled and perpendicular parking along the south side would be converted to parallel parking to accommodate an adaptive sidewalk along the frontage. Adaptive sidewalks are a facility type that have been piloted and implemented in various jurisdictions. They are often used where there is no existing sidewalk or where it may be more challenging to construct a raised concrete sidewalk due to physical or budgetary constraints. This would result in the loss of approximately 6 spaces.
- Removing angled parking reduces potential conflicts with active transportation users and allows for the narrowing of the travel lane, which helps facilitate lower vehicle speeds.

### What We Heard

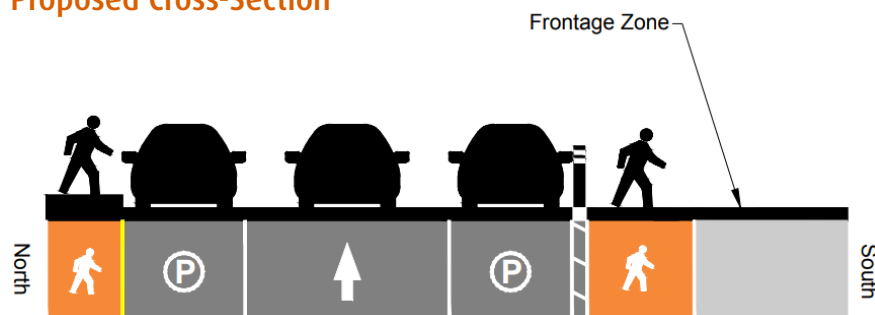
In the second survey, we asked the community *“What is your level of support for seeing new pedestrian facilities on McPhillips Avenue?”*

Over **81%** of survey respondents indicated they strongly support or support the proposed cross-section.

### Existing Cross-Section



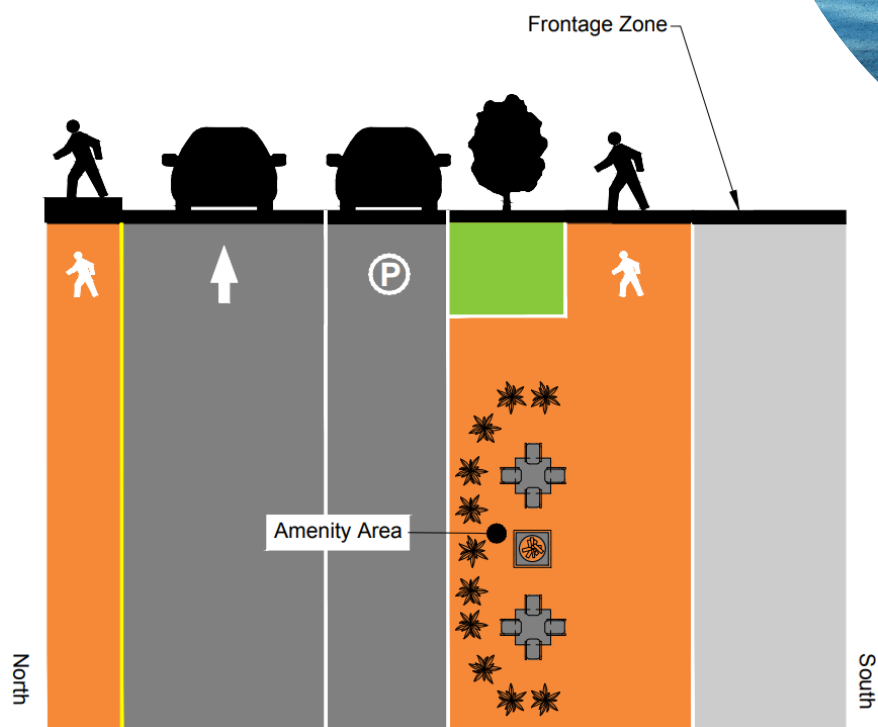
### Proposed Cross-Section



The proposed corridor improvements to McPhillips Avenue are anticipated to improve pedestrian safety and increase overall walkability within the village. To further pedestrianize the corridor and to help meet the guiding principle of a “pedestrian friendly village”, the CRD could work with MOTI to create an amenity area along the south side of the road in the summer months (i.e., tourism high season) for a select number of days or weeks. The cross-section below illustrates what this seasonal treatment could look like. There would be a loss of approximately 15 parking stalls in exchange for a public amenity area that provides an opportunity for residents and visitors to gather, rest, and socialize—all of which is in line with the guiding principle of creating a pedestrian friendly village.

Further, the proposed improvements to McPhillips Avenue could be completed simultaneously with the new off-street parking lots recommended at ArtSpring (see Action 3B).

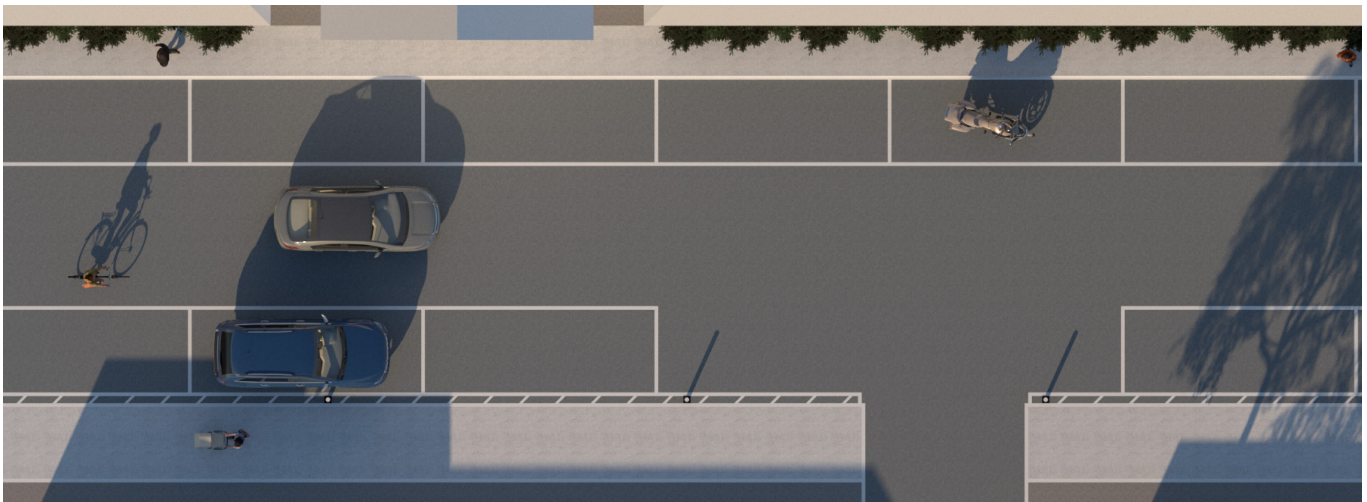
### Proposed Cross-Section for Seasonal Activation





# 1A McPhillips Avenue

## Option 1 - Improved Pedestrian Facilities



Sidewalk

Parallel Parking

Travel Lane

Parallel Parking

Sidewalk

\*Conceptual images for indication only



# 1A McPhillips Avenue

## Option 2 - Pedestrian Facilities + Planting & Amenity Zone



Sidewalk

Travel Lane

Parallel Parking

Planting & Amenity  
Zone

Sidewalk

\*Conceptual images for indication only

## Action 1B Implement Rainbow Road Pedestrian and Cycling Facilities

Rainbow Road serves as the main east-west corridor within Ganges Village and provides connections to other parts of Salt Spring Island.

The existing cross-section is shown below and is summarized as follows:

- Between Jackson Road to Lower Ganges Road, there is an existing concrete curb sidewalk on the north side and a shoulder on the south side where informal parking occurs.
- The sidewalk transitions to an at-grade gravel pathway along the north side of Rainbow Road west of Jackson Road until Windsor Plywood where a combination of parallel and perpendicular on-street parking is available.
- West of Windsor Plywood, the pedestrian facility continues as an at-grade gravel pathway with a ditch separating the facility from the motor vehicle travel lane.

Given the length of the Rainbow Road corridor, the proposed corridor improvements are presented in three segments in the following sections. It is recommended that improvements to all three segments occur at the same time.





## 1B-1

### From Jackson Road to Lower Ganges Road

Rainbow Road serves as the main east-west corridor within Ganges Village and provides connections to other parts of Salt Spring Island.

The proposed cross-section for Jackson Road to Lower Ganges Road and is summarized as follows:

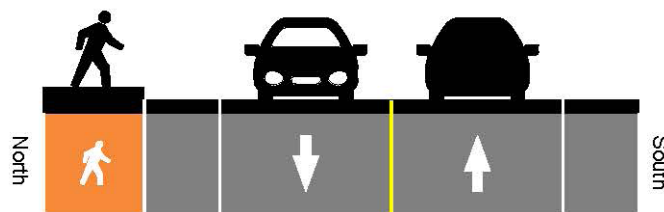
- The existing concrete curb sidewalk on the north side is retained.
- The shoulder on the north side is formalized into a painted bicycle lane (with pavement markings and signage).
- The existing shoulder on the south side of the road is widened to accommodate an adaptive sidewalk / pathway with flexible delineator posts or a rubber curb. The facility, recommended to be 2.0m in width, would be intended to serve people walking, rolling, and cycling. Even though there may be some user conflicts in the busier tourism season, people cycling could choose to take the road for this short stretch to access Lower Ganges Road.
- Even though parking is not allowed on the south side of the road, the recommended changes would prevent vehicles from continuing to park there in the future. The existing uses on the south side are mixed including a mix of commercial and institutional. A new pedestrian facility will make it easier for people walking and rolling to access these destinations.

### What We Heard

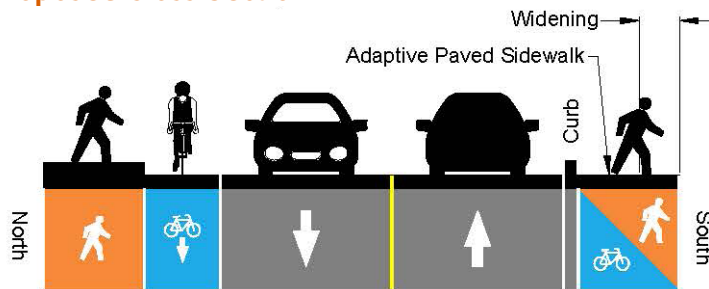
In the second survey, we asked the community *“What is your level of support for seeing new pedestrian and cycling facilities on Rainbow Road?”*

Over 88% of survey respondents indicated they strongly support or support the proposed cross-section from Jackson Road to Lower Ganges Road.

#### Existing Cross-Section



#### Proposed Cross-Section



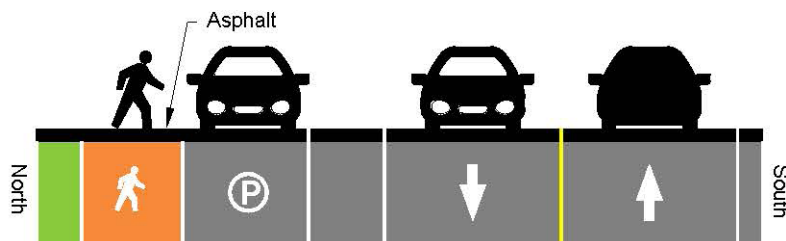
## 1B-2

### From Kanaka Road to Windsor Plywood

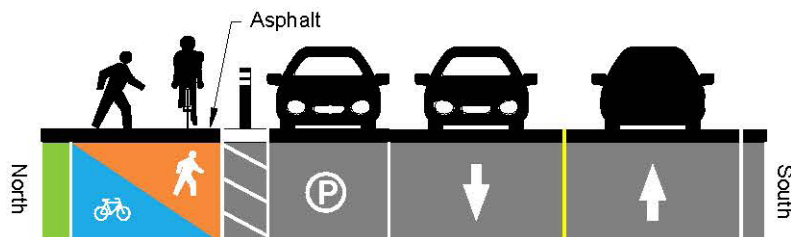
The proposed cross-section for Kanaka Road to Windsor Plywood and is summarized as follows:

- The existing gravel path is widened to a multi-use pathway to be shared by those walking, rolling, and cycling to prevent informal parking.
- A buffer with bollards and/or flexible delineator posts is provided along the Windsor Plywood frontage to provide additional separation from the parking lane. No separation is provided along the SD64 Maintenance Centre due to limited right-of-way.
- There are no anticipated impacts to on-street parking; therefore, the parking supply would remain as is.
- To help mitigate user conflicts on the multi-use pathway, the CRD could install signage and pavement markings to indicate clearly that the pathway is to be shared among different active mode users.

#### Existing Cross-Section



#### Proposed Cross-Section



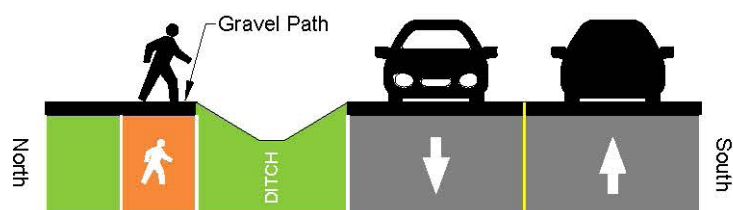


## 1B-3

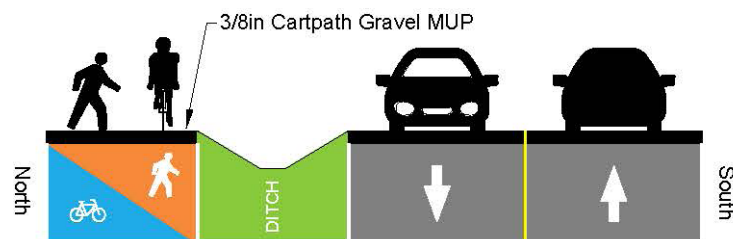
### West of Windsor Plywood

As shown below, the proposed cross-section west of Windsor Plywood is to widen the existing gravel path into a wider multi-use pathway to be shared by those walking, rolling, and cycling.

#### Existing Cross-Section



#### Proposed Cross-Section



## Action 1C Implement Fulford Ganges Road Pedestrian and Cycling Facilities

Fulford Ganges Road serves as one of the critical gateways into Ganges Village. According to MOTI, the motor vehicle volumes on Fulford-Ganges Road are in the range of 2,950-3,050 vehicles per day based on a July 2022 count including nearly 7 to 10% as heavy vehicles. And for some segments of the road, including within Ganges Village, the average AM vehicle speeds were 10% or more above the posted speed limit. Based on these data, protected pedestrian and cycling facilities are required for this corridor to improve road safety for all users and to increase overall comfort for active transportation users.

The existing cross-section for this corridor varies depending on the location; however, the area of focus within Ganges Village is from Seaview Avenue to Lower Ganges Road and Lower Ganges Road to Purvis Lane.

- **Seaview Avenue to Lower Ganges Road:**  
The existing cross-section for this segment includes a separated sidewalk with parallel parking along the north side of the road. The south side includes parallel parking with no pedestrian or cycling facilities.
- **Lower Ganges Road to Purvis Lane:**  
There is currently angled parking on both sides of the road. There is a sidewalk on the north side and a pedestrian pathway on the south side.

### What We Heard

In the second survey, we asked the community *“What is your level of support for new pedestrian and cycling facilities on Fulford - Ganges Road?”*

Over **78%** of survey respondents indicated they strongly support or support the proposed cross-section from **Seaview Avenue to Lower Ganges Road**.

Over **70%** of survey respondents indicated they strongly support or support the proposed cross-section from **Lower Ganges Road to Purvis Lane**.

*“Vehicles, bikes and people do not share lanes well. Separation is needed.”*

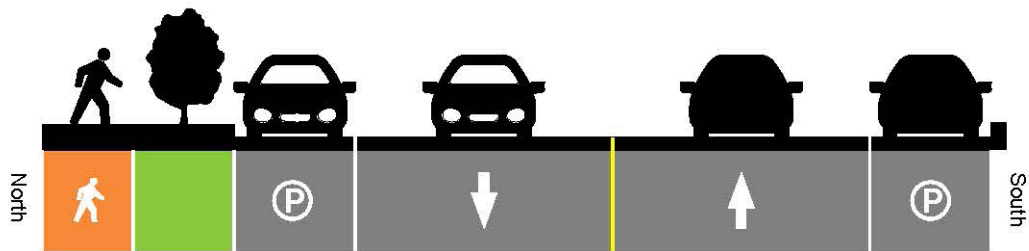
## 1C-1

### From Seaview Avenue to Lower Ganges Road

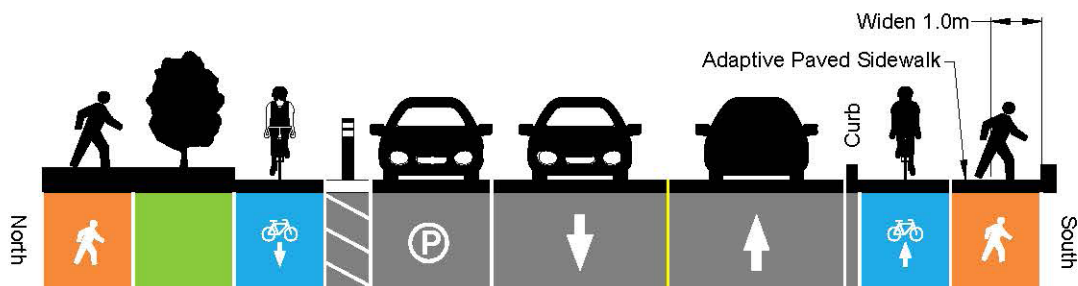
The proposed cross-section for Seaview Avenue to Lower Ganges Road is shown below and is summarized as follows:

- The existing concrete curb sidewalk on the north side is retained.
- The existing vehicle travel lanes would be reduced from 5.1m to 3.5m.
- The existing on-street parking on the north side is retained.
- A protected bicycle lane is provided along the north side of the road separated by bollards and/or flexible delineator posts.
- An adaptive sidewalk and protected bicycle lane is provided on the south side, which results in the loss of nine on-street parking spaces.
- A pedestrian refuge island and crosswalk is added at Seaview Avenue to slow vehicles down and serve as one of the “gateways” into the village to signal to drivers that they are entering an area with higher pedestrian activity.

#### Existing Cross-Section



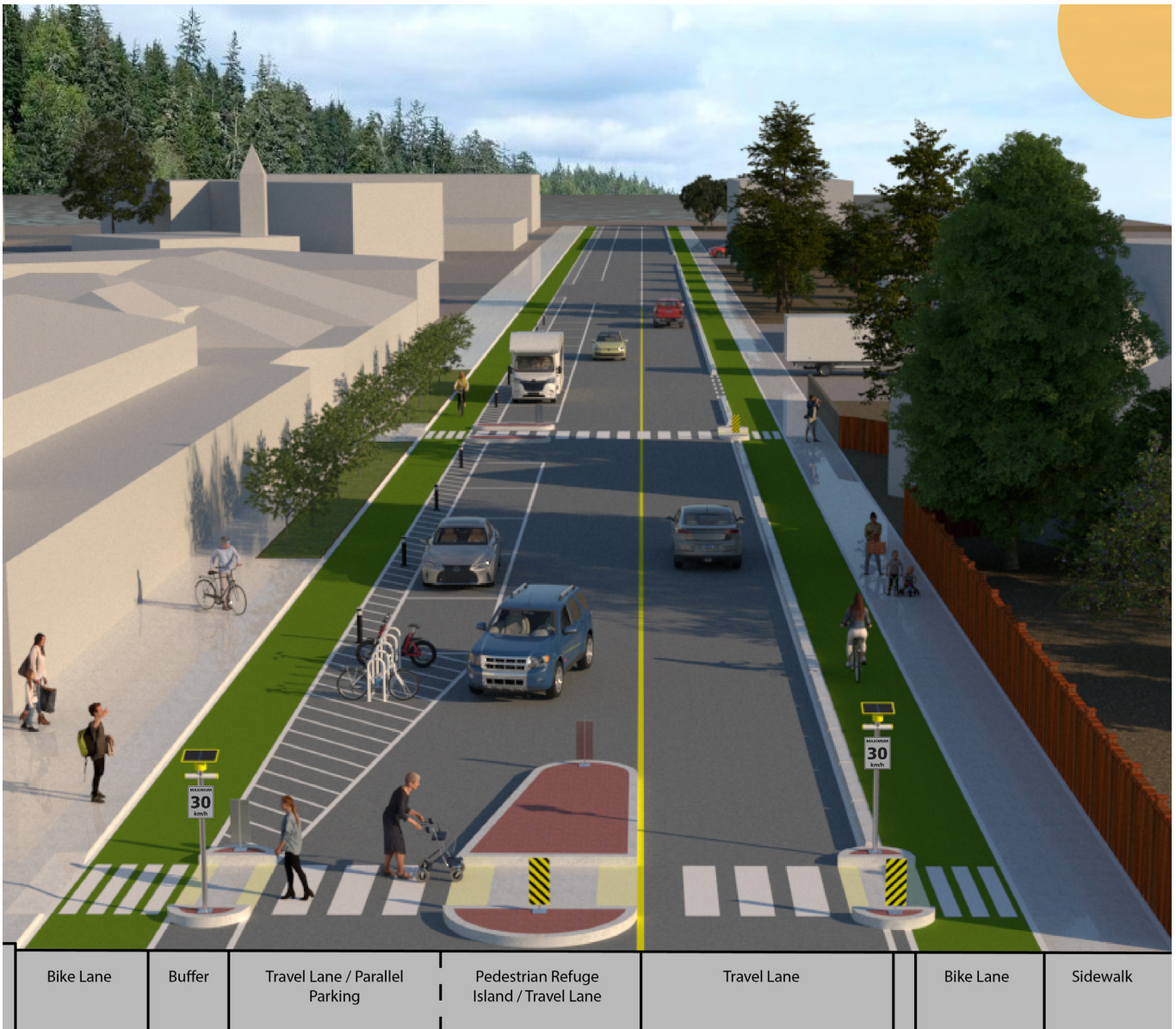
#### Proposed Cross-Section





# 1C-1 Fulford-Ganges Road

## Improved Pedestrian & Cycling Facilities



Bike Lane

Buffer

Travel Lane / Parallel  
Parking

Pedestrian Refuge  
Island / Travel Lane

Travel Lane

Bike Lane

Sidewalk

\*Conceptual images for indication only





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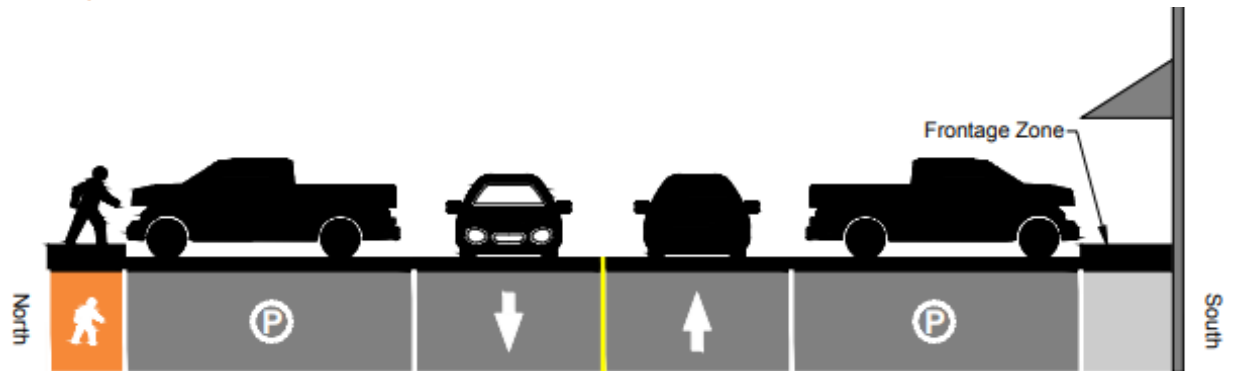
## 1C-2

### From Lower Ganges Road to Purvis Lane

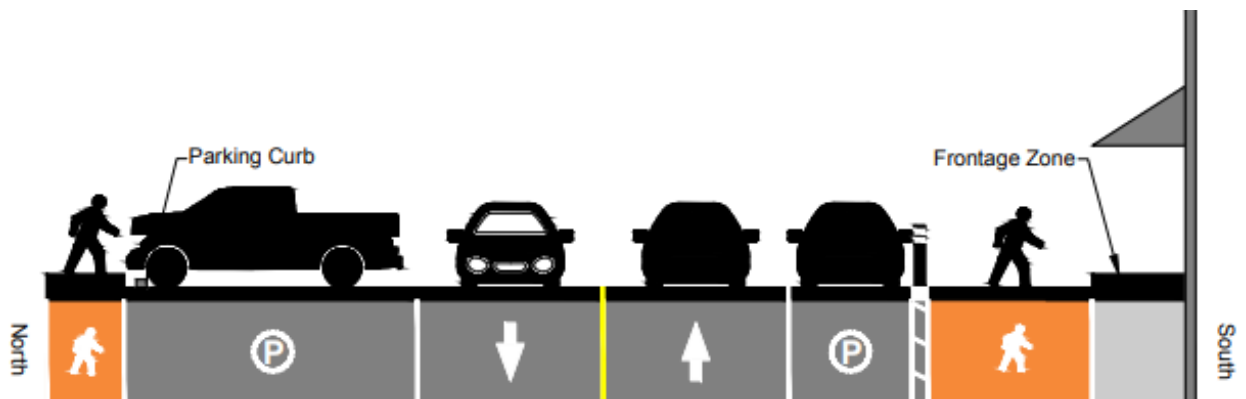
The proposed cross-section for Lower Ganges Road to Purvis Lane is shown below and is summarized as follows:

- The existing vehicle travel lanes would be reduced from 3.7m to 3.5m.
- The angled parking along the south side is converted to parallel parking to accommodate an adaptive sidewalk. This would provide more space for people walking and rolling. This would result in the loss of approximately 7 parking stalls on the south side.
- Wheel stops (parking curbs) are provided consistently in the angled parking spaces along the north side of the road to prevent parked vehicles from encroaching on the sidewalk.

#### Existing Cross-Section



#### Proposed Cross-Section



## Action 1D Implement Lower Ganges Road Pedestrian and Cycling Facilities

Like Fulford Ganges Road, Lower Ganges Road serves as the other critical gateway into Ganges Village. The existing cross-section for this corridor varies depending on the location; however, the area of focus within Ganges Village is from Fulford Ganges Road to Rainbow Road and Rainbow Road to Upper Ganges Road.

- **Fulford Ganges Road to Rainbow Road:**  
This section of the corridor has existing pedestrian facilities (concrete sidewalks) on both sides with limited on-street parking and a bus stop on the east side of the road. There are no cycling facilities provided. There is also a left-turn lane to facilitate turning movements to the east-west streets.
- **Rainbow Road to Upper Ganges Road:**  
This section of the corridor has an existing pedestrian facility (concrete sidewalk) and a painted bicycle lane on the west side of the road. The right-of-way varies considerably along the east side of the road with a shoulder, on-street parking for the Marina, and informal parking in some locations.

### What We Heard

In the second survey, we asked the community *“What is your level of support for new pedestrian and cycling facilities on Lower Ganges Road?”*

Over **68%** of survey respondents indicated they strongly support or support the proposed cross-section from **Fulford-Ganges Road to Rainbow Road**.

Over **84%** of survey respondents indicated they strongly support or support the proposed cross-section from **Rainbow Road to Upper Ganges Road**.

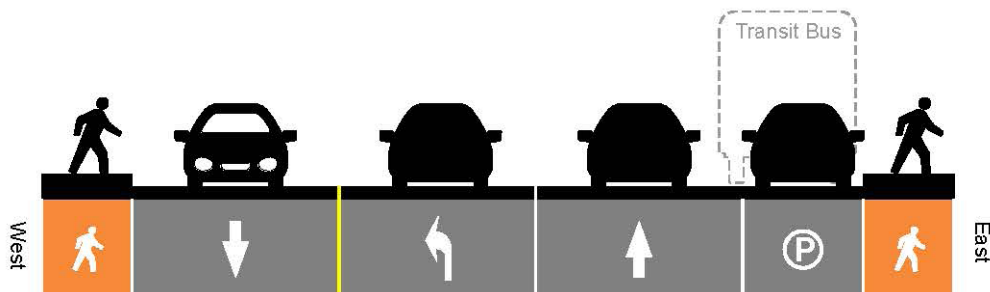
## 1D-1

### From Fulford-Ganges Road to Rainbow Road

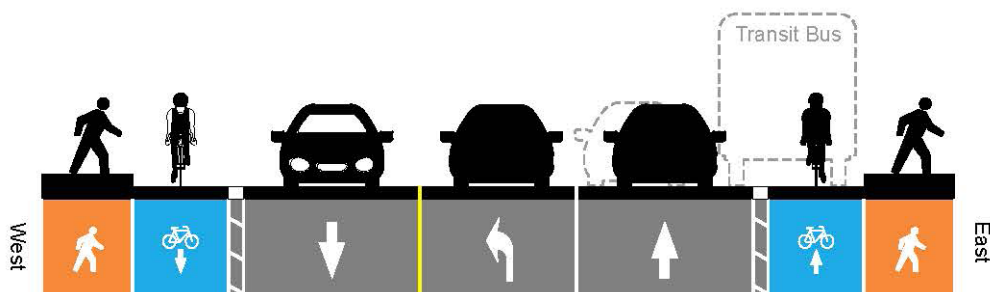
The proposed cross-section for Fulford-Ganges Road to Rainbow Road is shown below and is summarized as follows:

- The existing left-turn lane is retained to minimize impacts to traffic operations within Ganges Village and to the larger road network.
- The existing vehicle travel lanes would be reduced from 4.1m to 3.5m and the left-turn lane would be reduced from 3.9m to 3.1m.
- Buffered bicycle lanes are provided on both sides of the road. This results in the loss of approximately nine on-street parking spaces on the east side of the road.
- The existing transit stop is located within the bike lane and the person cycling will need to maneuver around the bus when it is stationed at the stop.
- Concrete curb sidewalks are constructed on both sides to fill in gaps and provide continuous facility coverage.

#### Existing Cross-Section



#### Proposed Cross-Section



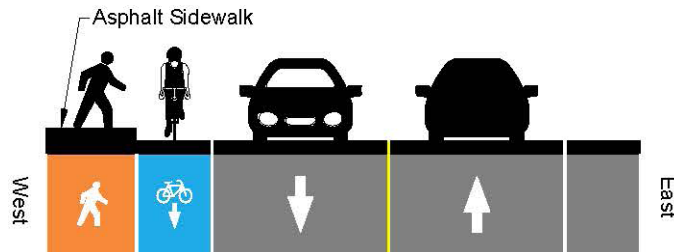


## 1D-2

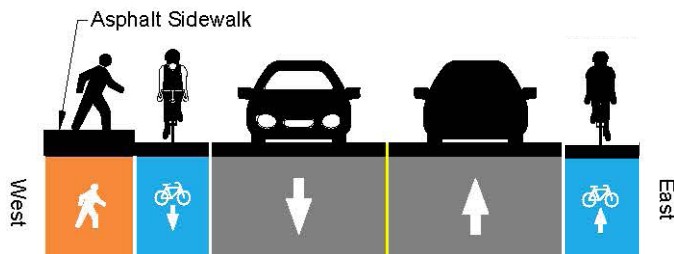
### From Rainbow Road to Upper Ganges Road (Interim)

In the interim, the shoulder on the east side is formalized into a painted bicycle lane (with pavement markings and signage). Implementing this change would result in people walking would no longer being able to walk along the shoulder on the and would be required to use the sidewalk on the west side of the road.

#### Existing Cross-Section



#### Proposed Cross-Section

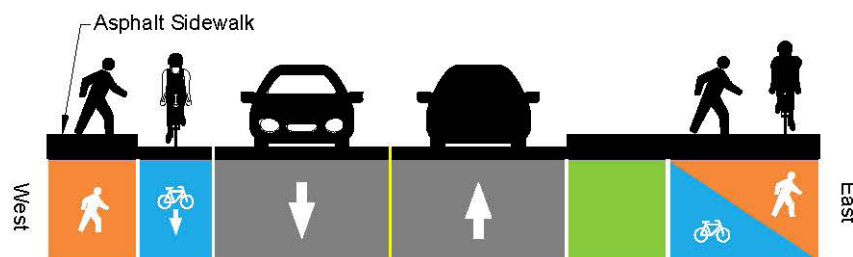


## 1D-3

### From Rainbow Road to Upper Ganges Road (Ultimate)

In the longer term, as part of enhancing overall safety and connectivity to/from Ganges Village, a more ultimate option should be pursued as shown below, which shows a multi-use pathway along the east side of the road. This would represent a much more significant capital cost and require property acquisition and retaining walls. This future facility also presents an opportunity to connect with the future Ganges Harbour Walk project, which can make the waterfront more accessible.

#### Proposed Cross-Section



## Action 1E Adopt Infrastructure Design Guidelines

As outlined above, several different facilities are proposed for the main corridors in Ganges Village. This section presents an overview of the infrastructure design guidelines for the proposed facilities.



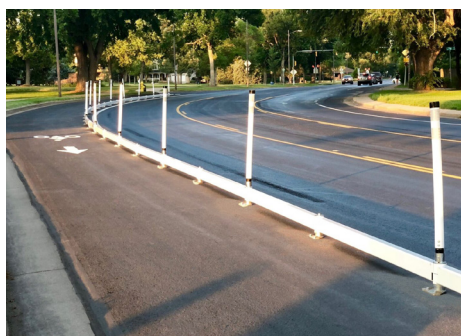
### Adaptive Sidewalk

Adaptive sidewalks are a facility type that have been piloted and implemented in various jurisdictions. They are often used where there is no existing sidewalk or where it may be more challenging to construct a raised concrete sidewalk due to physical or budgetary constraints. They typically include a separation treatment such as rubber curbs, flexible delineator posts, or something more permanent in nature such as bollards.



### Multi-use Pathways

Multi-use pathways (MUPs) are typically off-street pathways that are separated from motor vehicle traffic and can be used by any active transportation user, including people walking, cycling, and rolling. MUPs typically accommodate bi-directional travel and are commonly shared spaces. Separation between people walking and rolling, and people cycling may be considered if there is a high volume of users and/or there have been conflicts between active transportation user groups.



### Protected Bicycle Lane (Uni-directional)

A designated lane for people cycling and other active transportation users that is physically separated from motor vehicle traffic and people walking and rolling. Uni-directional facilities are the most suitable facility for the Ganges Village context. According to the BC Active Transportation Design Guide (BCATDG), uni-directional facilities should have a minimum width of 1.8m with a desirable width of 2.5m.



### Painted Bicycle Lane (including buffer)

Separate travel lanes designated exclusively for bicycle use that are delineated by a painted line and, in some cases, a painted buffer area. What makes the facility buffered is the additional separation between the bicycle lane and the motor vehicle travel lane and/ or parking lane by way of an additional white longitudinal line that runs parallel to the bicycle lane.

## Design Guidance for Proposed Facilities

Facility Type	Intended for	Design Details
Adaptive Sidewalk	Walking, Cycling, Rolling	<p><b>Width</b> – 1.8-2.0m (desirable), 1.5m (constrained)</p> <p><b>Surface Material</b> – Asphalt provides a smooth surface that is accessible for all user groups.</p> <p><b>Separation Treatment</b> – Flexible delineator posts, rubber curbs, and/or raised landscaped median.</p>
Multi-use Pathway	Walking, Cycling, Rolling	<p><b>Width</b> – 3.0m (desirable), 2.7m (constrained)</p> <p><b>Surface Material</b> – Asphalt provides a smooth surface that is accessible for all user groups.</p> <p><b>Signage</b> – Shared pathway sign (MUTCDC RB-93), which indicates that both people walking and cycling are allowed to use this facility.</p>
Protected Bike Lane	Cycling	<p><b>Width</b> – 2.0m (desirable), 1.5m (constrained)</p> <p><b>Separation Treatment</b> – Flexible delineator posts, precast curbs, and/or raised landscaped median.</p> <p><b>Signage</b> – Reserved bicycle lane sign (MUTCDC RB-90, RB-91) should be placed along protected bike lanes. The reserved bicycle lane ends sign (MUTCDC RB-92) should be placed where bike lanes end.</p>
Painted Bike Lane	Cycling	<p><b>Width</b> – 2.0m (desirable), 1.5m (constrained)</p> <p><b>Buffer</b> – 0.6m (desirable), 0.3m (constrained)</p>

More detailed guidelines for cycling facilities are found in Chapter D of the BC Active Transportation Design Guide

## Action 1F Connect to Active Transportation Network Outside of Ganges Village

The recommended corridor improvements within Ganges Village will enhance safety, connectivity, and overall accessibility for active transportation users once implemented. However, these improvements alone will not be sufficient to facilitate active transportation travel to/from Ganges Village. As such, the CRD, in collaboration with MOTI, will need to continue to identify, prioritize, and implement improvements along the island's major roadways to help meet the vision and guiding principles of the ATNP and the recommendations in the Salt Spring Island Cycling Safety Review.

The Salt Spring Island Cycling Safety Review identifies the lack of continuous cycling facilities along the island's three major corridors including Vesuvius Road, Lower Ganges Road, and Fulford-Ganges Road. The report identifies several cycling facility options for these corridors and ultimately recommended that one option be pursued in the short-term—bicycle accessible shoulders. The BC Active Transportation Design Guide recommends an absolute minimum shoulder width of 1.2 metres based on the horizontal operating envelope of a person cycling. However, the authors of the Spring Island Cycling Safety Review note that an absolute minimum shoulder width of 1.0 metres would be supported given the rural nature of Salt Spring and other physical constraints including the limited right-of-way.

In alignment with the Spring Island Cycling Safety Review, it is recommended that the CRD, in collaboration with MOTI, prioritize the following to enhance connectivity to Ganges Village. Implement bicycle accessible shoulders along the following segments:

1. Fulford-Ganges Road (from Beddis Road to Seaview Avenue)
2. Lower Ganges Road (from Booth Canal Road to Atkins Road)



## Actions & Indicators Summary

Strategy Area 1 Actions:	
1A	Implement McPhillips Avenue pedestrian facilities
1B-1	Implement Rainbow Road pedestrian and cycling facilities (from Jackson Road to Lower Ganges Road)
1B-2	Implement Rainbow Road pedestrian and cycling facilities (from Kanaka Road to 166 Rainbow Road)
1B-3	Implement Rainbow Road pedestrian and cycling facilities (west of Windsor Plywood)
1C-1	Implement Fulford Ganges Road pedestrian and cycling facilities (from Seaview Avenue to Lower Ganges Road)
1C-2	Implement Fulford Ganges Road pedestrian and cycling facilities (from Lower Ganges Road to Purvis Lane)
1D-1	Implement Lower Ganges Road pedestrian and cycling facilities (from Fulford-Ganges Road to Rainbow Road)
1D-2	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Interim Solution)
1D-3	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Ultimate Solution)
1E	Adopt infrastructure design guidelines
1F	Connect to active transportation network outside of Ganges Village
Measuring the Actions	
Total length (metres) of new pedestrian facilities	
Total length (metres) of new cycling facilities	
The number of pedestrian and cycling collisions	

## 6.0 Strategy Area 2

# Make Ganges Village Accessible for all Residents and Visitors

Strategy area 2 includes a total of 6 actions that can be pursued to improve accessibility and road safety and help Ganges become a more pedestrian friendly village. Most of Salt Spring Island's sidewalk network is concentrated around Ganges Village, however, many sidewalks and curb letdowns in the Village are not accessible for the elderly, those using strollers / baby carriages, and those with mobility aids.

Most of Salt Spring Island's sidewalk network is concentrated around Ganges Village, however, many sidewalks and curb letdowns in the Village are not accessible for the elderly, those using strollers / baby carriages, and those with mobility aids.

There are many examples of pedestrian facilities on the island with deficient curb ramps, also referred to as "curb cuts" and "sidewalk letdowns". They are required for people using wheelchairs, power scooters, and other mobility devices, but also benefit people with strollers, baggage, and delivery carts. They are also used as a navigational tool by people with visual impairments. Many of the curb ramps on the island do not meet best practices in the BC Active Transportation Design Guide.

Accessibility is a particular challenge within Ganges Village, where surface parking lots, haphazard parking during peak tourism months, and access to businesses and store fronts present accessibility challenges. Additionally, Most of the sidewalks or pathways have insufficient widths for those using a mobility aid to pass safely and comfortably.

The following key themes emerged from public engagement and technical analysis:

1. Missing pedestrian connectivity to key destination
2. Missing or insufficient curb ramps
3. Existing pedestrian facilities have uneven surfaces
4. Narrow or obstructed pedestrian through zones for passing or mobility aids
5. On-street parking overhanging and obstructing the pedestrian through zone
6. Violations of no-parking zones obstructing curb ramps or pedestrian through zone
7. Insufficient pavement/gravel conditions along sidewalks/shoulders



## What We Heard

*"We need proper sidewalks and ramps to accommodate our disabled community members, parents with strollers and the elderly."*

*"As a pedestrian the harbour area and Ganges village is very unfriendly. The parking patterns are all over the place and traffic patterns as well. Please consider closing down some routes from vehicles altogether - and encourage the culture of a car free community."*

## Action 2A Conduct a Land Ownership Survey of Ganges Village

A major challenge in addressing active transportation infrastructure improvements and accessibility challenges in Ganges Village is the fragmented nature of jurisdictional boundaries and ownership. The Capital Regional District and the Local Community Commission cannot continue to rely on property owners to address accessibility barriers along their frontages. To identify missing, substandard, or insufficient pedestrian infrastructure, the CRD—with its associated partners—must conduct a comprehensive land ownership survey of Ganges Village to identify the ownership of properties, including those owned by the Ministry of Transportation Infrastructure, private owners, and commercial entities.

A land ownership survey would allow the CRD to undertake a more formal assessment of the condition of the sidewalks, pathways, crosswalks, and curbs to determine legal responsibility, resource requirements, and subsequently be in a position to develop design specifications and maintenance standards. Specifically, it is recommended that the CRD investigate options related to tenure as part of completing the land ownership survey.

This action would serve as an important first step in addressing accessibility challenges in the village, while reducing the reliance on property owners to undertake such improvements.





## Action 2B Adopt a 30 km/h Posted Speed Limit in Ganges Village

Throughout the community engagement process, the issue of high vehicle speeds emerged as one of the top barriers for walking and cycling on Salt Spring today. Currently, Ganges Village has a 50 km/h posted speed limit. Reducing the posted vehicle speed limit—and thereby reducing vehicle speeds—can be effective at improving overall road safety by preventing crashes and reducing the severity of injuries.

Significant safety concerns can arise due to high operating speeds and differential speeds between drivers and vulnerable road users such as pedestrians and cyclists. Motor vehicles moving at high speeds pose a greater risk to vulnerable road users, who are at a heightened risk of severe injury or even death in the event of a collision. When drivers are traveling at lower speeds, they have more time to react to unexpected events or hazards in the road. This can help them avoid collisions with pedestrians or cyclists who may suddenly enter the roadway.

One way of achieving lower speeds is through adopting a 30 km/h posted speed limit within Ganges Village. A 30 km/h zone can create a more pleasant and pedestrianized village environment for residents and visitors alike. Further, there are several co-benefits associated with reduced operating speeds including reduced noise levels and a reduction in collisions.

The CRD should work closely with MOTI to create a 30 km/h posted speed limit within Ganges Village to improve both pedestrian and cycling safety, which is consistent with the findings and recommendations in the Salt Spring Island Cycling Safety Review.

Overall, the successful implementation of a 30 km/h zone in Ganges Village will require installing appropriate signage and road markings to clearly indicate the reduced speed limit as well as supporting an awareness campaign to educate residents and visitors about the new speed limit and the importance of reducing speed in areas with high pedestrian and cycling activity.

### What We Heard

In the second survey, we asked the community *“What is your level of support for implementing a 30 km/h zone within the Ganges Village?”*

Over **82%** of survey respondents indicated they strongly support or support the implementation of a 30km/h zone within the Ganges Village.

## Action 2C Create Village Gateways with Traffic Calming Design Interventions

The lowering of posted speeds should be accompanied by traffic calming design interventions to have a more effective impact in slowing down vehicles. The ATNP envisions “gateways” in and out of the Ganges Village to better signal to drivers that they are entering an area with higher pedestrian activity. Two gateways to Ganges Village are recommended at the following locations:

1. Fulford-Ganges Road / Seaview Avenue (southern gateway)
2. Lower Ganges Road / Rainbow Road (northern gateway)

Specifically, the following treatments are recommended for these two gateways:

- Pedestrian refuge island, which reduces the exposure time of a person walking in the intersection. They narrow the roadway and can help increase overall comfort and safety for people walking.
- Curb extensions can visually and physically narrow the roadway. They help create safer and shorter crossings for people walking. They also increase the overall visibility of people crossing the roadway by aligning them with the parking lane and reducing overall crossing distance.
- 30 km/h signage would be posted at the start of the gateways.
- Pedestrian flashers, which would enhance warning and awareness for motorists of a crosswalk.
- All the proposed treatments above are conceptual in nature; the exact traffic calming treatment will be determined in the detailed design stage once these intersections are rebuilt.



Examples of pedestrian refuge islands in Esquimalt (top) and Crofton (bottom).

### What We Heard

In the second survey, we asked the community “What is your level of support for implementing traffic calming measures at the gateways entering Ganges Village?”

Over 84% of survey respondents indicated they strongly support or support the implementation of traffic calming measures at the Village gateways.

## Action 2D Undertake Reviews of Intersections within Ganges Village

As outlined in the BC Active Transportation Design Guide, intersection design is a critical part of both pedestrian and cycling facility design. The provision of safe and accessible crossings makes it easier for people of all ages and abilities to move around their community to reach their destination. Similarly, active transportation network planning must consider how people cycling can navigate intersections in a safe and comfortable manner.

Based on the public engagement feedback and the technical analysis completed in the development of the ATNP, the CRD, in collaboration with MOTI, should undertake intersection improvement reviews that could lead to changes related to signage, pavement markings, intersection geometry, vehicle speeds, and the elimination of conflicts with other roadway users. For example, this could include the installation of Rectangular Rapid Flashing Beacons at uncontrolled approaches or protected cycling infrastructure up to the edge of the intersection.

The following locations are recommended for a review. More detailed guidance on intersection and crossing treatments is available in Chapter G of the BC Active Transportation Design Guide.



## 2D-1 Fulford-Ganges Road and Lower Ganges Road

This intersection serves the primary movement of cars to and from Lower Ganges Road to Fulford-Ganges Road. The current configuration of the intersection is complex with unclear right-of-way and accessibility challenges.

Two concepts were developed as part of the Salt Spring Island Cycling Safety Review. In the interim, it is recommended that the intersection be realigned to a 90-degree angle with stop signs and crosswalks on all legs. In the long run, an alternative proposal suggests constructing a single-lane roundabout, but this would depend on the planned relocation of the fire hall that is situated near the current intersection.

### Existing Conditions



### Recommendations from the Salt Spring Island Cycling Safety Review





### 2D-2 Fulford-Ganges Road and Purvis Lane

Immediately adjacent to parking, this intersection has poor pedestrian visibility, poor sightlines, and unclear right-of-way. On-street parking near the crosswalk should be potentially removed to address these issues.



### 2D-3 Lower Ganges Road and McPhillips Avenue

Diagonal marked crosswalk with no dedicated pedestrian facility, which presents accessibility challenges for those with mobility aids. When the proposed corridors improvements are made to Lower Ganges Road, specific intersection treatments that could be implemented at this location including conflict zone markings and signage to indicate limited sightlines.



### 2D-4 Lower Ganges Road and Herford Avenue

Example of a location with deficient curb ramps. Where feasible, the recommended approach per the BC Active Transportation Design Guide is to provide double curb ramps. Double curb ramps help to provide full universal access by landing pedestrians directly in the crossing area and in the desired direction of travel, rather than entering the road at an angle and having to reorient themselves. This is especially important for pedestrians using mobility devices and who are visually impaired.



### 2D-5 Lower Ganges Road and Rainbow Road

This location is proposed as one of the "gateways". The intersection currently has a pedestrian-activated amber flasher. Due to its proximity to Salt Spring Elementary School, it should be a candidate for additional safety improvements including traffic calming measures such as a pedestrian refuge island with curb extensions. It is important to consider pedestrian safety for the Southern Gulf Island school children arriving by water taxi at Ganges who are walking from the docks to Rainbow Road across Lower Ganges Road.





## 2D-6 Lower Ganges Road and Upper Ganges Road

This intersection is located at the bottom of a hill on Lower Ganges Road with long crossing distances and limited pedestrian visibility. The Salt Spring Island Cycling Safety Review recommends realigning the location of stop bar and adding Rectangular Rapid Flashing Beacon or similar device for the crosswalk across Lower Ganges Road to improve pedestrian visibility and safety.

### Existing Conditions



### Recommendations from the Salt Spring Island Cycling Safety Review



## Action 2E Retain and Retrofit Accessible Parking within the Village

As outlined in Strategy Area 1, several of the recommended corridor improvements would result in the loss of some on-street parking in exchange for active transportation facilities. This is a tradeoff and something many other communities in BC are having to contend with. In the Ganges Village context, where there is an opportunity to achieve a pedestrianized village, utilizing parking space for active transportation is a cost-effective solution compared to moving curbs.

However, the removal of all on-street parking is not practical as some users require parking at their destination. As such, all existing accessible parking spaces should be retained (where possible). By retaining accessible parking as part of the Salt Spring Island Active Transportation Network Plan, the community can ensure that people with mobility impairments have access to buildings and services in the community. This is particularly important given the island's aging population and can include those who:

- need the extra width of the designated spaces to get in and out of their vehicles in a wheelchair;
- use crutches, a cane or other mobility aid, or
- need to park close to a building entrance because their health prevents them from walking very far.

Retaining and providing accessible parking options as part of the Active Transportation Network Plan is an essential step towards creating a truly inclusive and accessible community. Therefore, it is recommended that the CRD first complete an inventory of the accessible parking stalls within the village. Then, it should ensure that all spaces be upgraded to a higher standard as they do not meet best practices. The following design standards are recommended:

### A. Design & Layout

- The length and width of the stall should have the same dimensions as a regular on-street stall. However, a no parking zone (access aisle) should be provided behind all accessible stalls and be a minimum of 1.8m in width.
- A curb ramp should align with the no parking zone (access aisle) to ensure there is an accessible path of travel from the road to the sidewalk.
- A clear 2m sidewalk/boulevard space should be provided adjacent to the side doors to make it easier for wheelchair users to enter/exit their vehicle using a side lift ramp.

### B. Curb

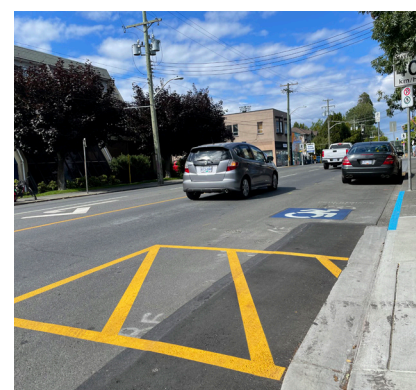
- All accessible spaces should have a blue painted curb.

### B. Signage

- A standalone vertical sign should be installed at each standard and van accessible parking space consistent with best practices.

### D. Pavement Marking

- The pavement marking should have a blue background with the new International Symbol of Access in white for high tonal contrast.





## Action 2F Establish No-parking Zones with Physical Barriers

Parking restrictions are intended to minimize an unwanted behaviour. For example, restrictions may be in place to prevent vehicles from blocking driveways, encroaching pedestrian facilities, and minimizing safety hazards. In areas where unregulated parking is an accessibility concern, it is essential to formalize no-parking zones with physical barriers, such as bollards or curbs, to prevent vehicles from parking in prohibited areas.

Due to the CRD's limited ability to enforce parking within Ganges Village, it is recommended that they work with MOTI to create no-parking zones whereby specific on-street parking spaces would be marked off with physical barriers to prevent vehicles from blocking these areas, which would reduce the risk of accidents, injuries, and other safety concerns.





## Actions & Indicators Summary

Strategy Area 2 Actions:	
2A	Conduct a land ownership survey of the Ganges Village
2B	Adopt a 30 km/h posted speed limit in Ganges Village
2C	Create village gateways with traffic calming design interventions
2D	Undertake reviews of intersections within Ganges Village
2E	Retain accessible parking within the Village
2F	Establish no-parking zones with physical barriers where necessary
Measuring the Actions	
Number of traffic calming devices installed	
Number of crashes resulting in serious injury or death of people who are walking or cycling	
Number of curb ramps meeting accessibility design guidelines	
Number of accessible parking spaces that meet best practices	

# 7.0 Strategy Area 3

## Encourage Sustainable Modes

In addition to the provision of appropriate infrastructure, a high-quality active transportation network must be supported by community programs, educational initiatives, amenities, maintenance, and policies to help facilitate culture change. This section outlines supportive active transportation initiatives and programs that the CRD could lead and/or coordinate with other organizations on Salt Spring Island.

## Action 3A Develop a Bike Parking Retrofit Program

Ganges Village is the commercial hub of Salt Spring Island, with a variety of shops, restaurants, and attractions. However, the lack of dedicated and high-quality bicycle parking facilities in this area can be a major deterrent for people cycling, as it can be challenging to find a safe and secure place to lock up their bikes.

Bicycle parking is currently available in the village, but the racks are often poorly maintained or located in inconvenient locations. Most of the racks are “coat hangers”, which are not recommended in the BC Active Transportation Design Guide (BC ATDG) as they do not accommodate all types of bicycles, especially oversized bicycles including electric cargo bikes and bikes with trailers. According to the BC ATDG, the ‘Inverted U’ and ‘Post and Ring’ rack types are recommended due to their versatile application. They can also accommodate oversized bicycles, but sufficient space is required to allow people cycling to park without obstructing pedestrian facilities or blocking access to private properties. Over time, there may other forms of micro-mobility on the island including e-scooters and therefore other parking design details will need to be considered such as proximity to electrical outlets for charging.

To help achieve better designed bike parking facilities, it is recommended that the CRD establish a bike parking retrofit program. The purpose of the program is two-fold: (1) allow the CRD to retrofit, over time, all its CRD-owned short-term bike parking facilities to align with the BC ATDG and (2) work with business and commercial property owners to encourage them to install or retrofit short-term bike parking to align with best practices. The CRD will need to explore different incentive structures to help get buy-in from the private sector, which could include tax breaks or other financial mechanisms.

### Additional Information

More detailed parking guidelines for oversized bicycles and e-scooters are found in Chapter H.5 of the BC Active Transportation Design Guide.

#### Bicycle Racks for All Applications

Inverted U

(Also called loop or staple rack)



Post and Ring



Image credit: BC Active Transportation Design Guide



Example of ‘Coat Hanger’ rack type which does not meet best practices



Image credit: Dublin City Council

## Action 3B Identify Opportunities to Increase Off-Street Parking

The implementation of new active transportation facilities will require the reallocation of road space and losses in on-street parking. Several on-street parking spaces will be lost once all the active transportation facilities are constructed. To mitigate these impacts, it is recommended that the CRD secure off-street parking facilities within Ganges Village to increase parking supply for market vendors, staff, visitors, and residents outside of the congested areas.

The CRD should undertake engagement and consultation with private and public entity landowners to determine the feasibility of using these areas for parking and specifically consider factors such as location, size, accessibility, and safety.

As shown in the map, there are three potential locations for dedicated off-street parking facilities that could be used as general parking for the village:

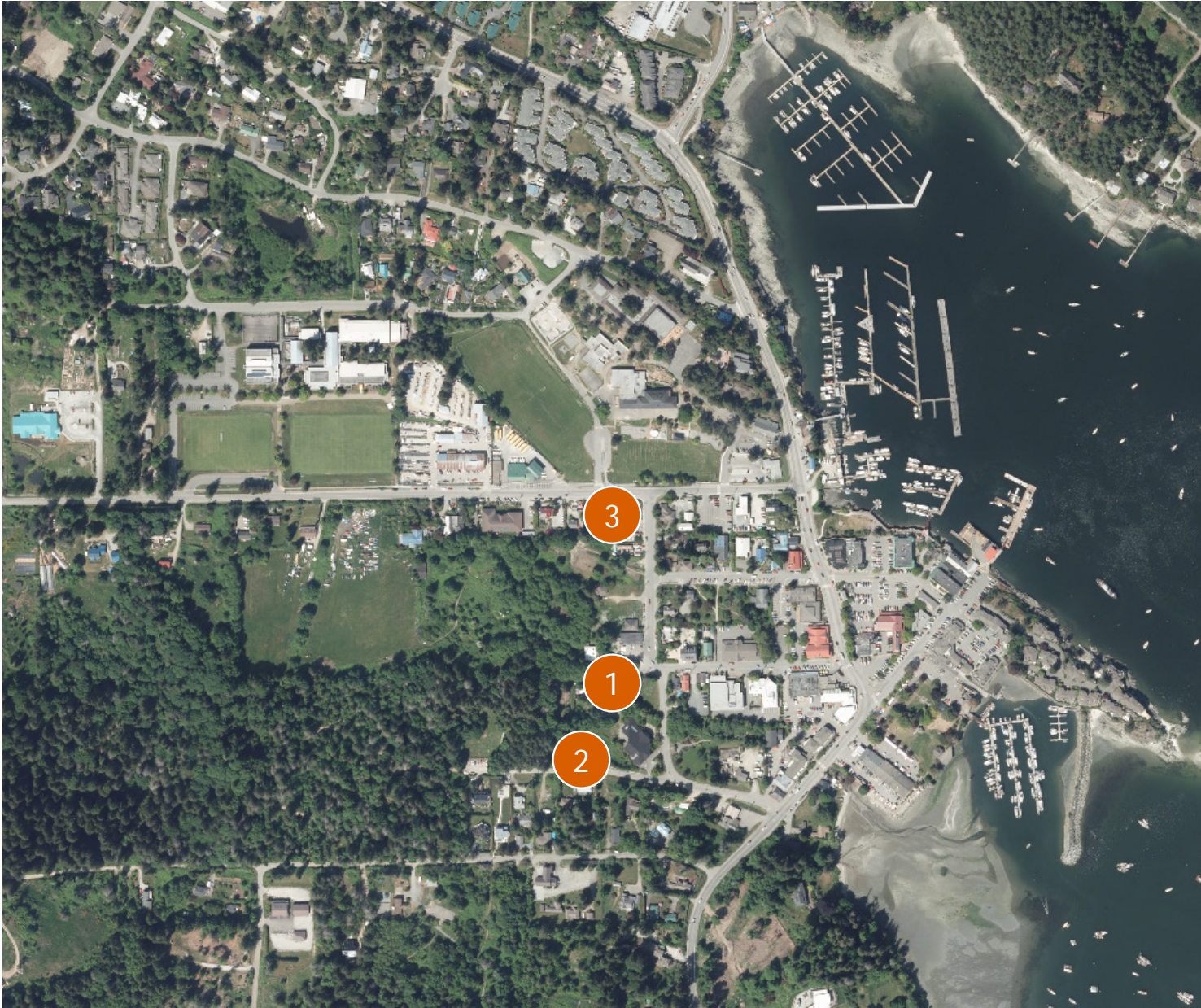
1. A row of parking stalls north of ArtSpring
2. The off-street lot immediately west of ArtSpring
3. A private lot along Rainbow Road near Jackson Avenue

The parking at ArtSpring could be utilized for off-street parking either on a temporary or permanent basis. It is recommended that the CRD negotiate with ArtSpring for the use of the off-street lots. Specifically, the CRD can discuss the possibility of leasing or renting these spaces for parking on “peak” parking demand day and agree to terms and conditions that include operational responsibility and cost. Further, the CRD should argue that both parking locations would facilitate a connection to the proposed improvements on McPhillips Avenue (Action 1A), which would provide more direct and safe connections for pedestrians trying access the core of the village.

A third location along Rainbow Road is also recommended, which would allow people to utilize the future active transportation facilities along this corridor to access various parts of the village.

Securing additional off-street parking would create more capacity and alleviate existing demands on on-street parking and the off-street lots in the village. This would help to alleviate parking congestion and traffic flow while making it easier for drivers to find available parking spaces. This would also help to better utilize the existing parking supply in Ganges Village and enhance the overall visitor experience.





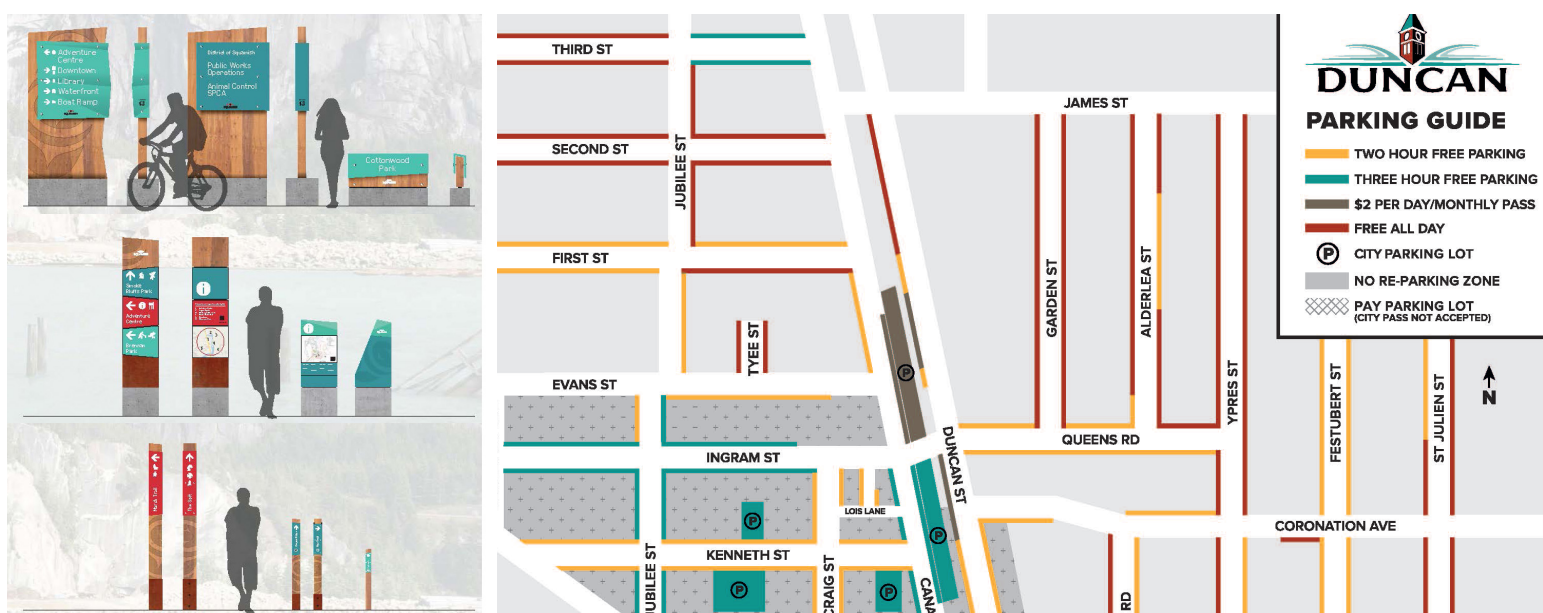
## Action 3C Develop a Parking Wayfinding Program

According to the 2018 Ganges Parking Study, there are approximately 1,156 parking spaces within the “Village Core”, which makes up most of the study area for the ATNP. Even though there are a significant number of parking spaces, visitors, especially on market days, may not know where to find parking. One way to address this challenge is to develop a parking wayfinding plan.

Developing a parking wayfinding plan is a crucial step towards optimizing parking in Ganges Village. Finding parking can be a frustrating experience for visitors and can negatively impact their overall experience in Ganges Village. By reducing the number of cars circling or “cruising” for an available parking space, overall traffic operations would improve, which would thereby reduce congestion and travel times. This would create a more efficient and safer environment for all road users.

It is recommended that the CRD determine the parking areas that will be included in the wayfinding plan and assess their signage needs, considering the location, accessibility, and capacity of each area. A comprehensive wayfinding system should be developed that includes maps, directional signs, and other visual cues to guide drivers to available parking areas. It is important to ensure that the system is user-friendly, easy to follow, and clearly communicates critical information such as time limits.

It is important to regularly evaluate the effectiveness of the parking wayfinding plan and make adaptations as necessary to ensure that it continues to meet the needs of the community over time.



Example of wayfinding / signage and a parking guide map



## Action 3D Work with BC Transit to Integrate Active Transportation Modes

Public transit is considered a form of active transportation as all transit trips begin and end with a person walking, cycling, or rolling. Therefore, better integrating transit with the active transportation network is essential for increasing overall active transportation mode share. Integration refers to the coordination and connectivity of different modes of transportation, including public transit, walking, and cycling. BC Transit provides bike racks on their buses to facilitate people to easily combine active transportation with public transit.

When transit systems are integrated with active transportation options, it becomes easier for people to combine different modes of transportation to reach their destinations or provide the option to transit part of the way or one-way to their destination. Transit also provides a weather-proof option for supporting active transportation by providing an alternative option in inclement weather such as rain or snow, people can choose to use public transit instead of walking or cycling to their destination.

One of the infrastructure priorities identified in the Salt Spring Island 2021 Transit Future Service Plan is to create two additional bays at the Ganges Exchange to accommodate increased peak service. As part of adding these bays to the Ganges Exchange, the CRD should work with BC Transit to ensure that additional amenities are included that support active transportation such as short-term bicycle parking racks, a bus shelter, and wayfinding / signage to direct transit users to key destinations in Ganges that are within walking distance.



## Actions & Indicators Summary

Strategy Area 3 Actions:	
3A	Develop a bike parking retrofit program
3B	Identify opportunities to increase off-street parking
3C	Develop a parking wayfinding program
3D	Work with BC Transit to integrate active transportation modes
Measuring the Actions	
The percentage of bicycle parking racks within Ganges Village meeting best practices	
The number of new bicycle parking racks installed	
The number of parking wayfinding signs installed	
The number of off-street parking stalls added	





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# 8.0 Implementation Strategy

## 8.1 Big Moves

A total of 26 actions are recommended in the ATNP. The implementation of each action will require time, resources, and financial commitments. The recommended sequencing of the actions is shown in Section 8.3 (Action Plan). While all the actions in the ATNP are critical for helping meet the plan's vision and targets, some are deemed to be of higher importance to have a more immediate and direct impact on Ganges' active transportation network.

The five big moves were selected based on meeting at least one of the following criteria:

### 1 Low-cost

In alignment with the first guiding principle of the ATNP, a big move should be relatively low cost recognizing the limited financial resources for infrastructure on the island.

### 2 Actionable

In alignment with the first guiding principle of the ATNP, a big move should be actionable, meaning that it requires minimal staff time and strong support from key stakeholders such as MOTI to make implementation seamless.

### 3 High Impact

High impact refers to increasing the number of people walking, cycling, and rolling while improving safety and accessibility for the most vulnerable road users.

The following "big moves" have been identified with their corresponding action number. The big moves have been listed based on their order of priority reflecting the criteria above. Ultimately, however, the CRD and its partners will need to determine what is most feasible to implement first based on funding opportunities, staffing, and overall resources.

### 8.1.1 Big Move 1

Adopt a 30km/h posted speed limit in Ganges Village (Action 2B)

#### What is the move?

Per Action 2B, the CRD should work closely with MOTI to create a 30 km/h posted speed limit within Ganges Village to improve both pedestrian and cycling safety, which is consistent with the findings and recommendations in the Salt Spring Island Cycling Safety Review.

#### Why is it important?

A 30 km/h posted speed limit within Ganges Village will help contribute to lower speeds and improved safety for active travelers while waiting for all the recommended active transportation facilities to be built. While changing the posted speed limit is an important first step to address speeding and road safety, additional traffic calming measures (Action 2C) will also be needed to address vehicle operating speeds.

### 8.1.2 Big Move 2

Implement McPhillips Avenue Pedestrian Facilities (Action 1A)

#### What is the move?

Per Action 1A, McPhillips Avenue is recommended to be upgraded with pedestrian facilities to help create a more pedestrian village in Ganges.

#### Why is it important?

The recommended corridor improvements to McPhillips Avenue are anticipated to improve pedestrian safety and increase overall walkability within the village. McPhillips Avenue serves as an important east-west corridor and improved pedestrian facilities will make it easier for people walking and rolling to access destinations along McPhillips and improve access to the core of the village. McPhillips does not experience the volume of vehicle and bicycle volumes or transit activity as Lower Ganges and Fulford-Ganges Road and therefore has great potential to be implemented as a quick-win project to enhance active transportation infrastructure.

### 8.1.3 Big Move 3

Improve the Parking Experience in Ganges Village (Actions 2E, 3B, 3C)

#### What is the move?

Per Action 2E, 3B and 3C, vehicle parking will continue to be an important amenity within Ganges Village. The loss of on-street parking associated with active transportation facilities presents an opportunity to improve existing accessible on-street parking and increase the supply of off-street parking.

#### Why is it important?

Some user groups, including seniors and persons with mobility impairments, require vehicle parking closer to their destination. As such, retaining and retrofitting existing accessible parking spaces is critical to meet their needs.

Similarly, the provision of more off-street parking can serve the needs of multiple user groups including those who may only require parking for a short period of time (e.g., customers, visitors), or a longer period (e.g., vendors, businesses, employees).

The objective of this Big Move is to enhance the parking experience in Ganges Village to alleviate community concerns about the ATNP's effects on parking. By doing so, it aims to increase community buy-in and support for the proposed active transportation facilities that require the removal of on-street parking spaces.

### 8.1.4 Big Move 4

Conduct a Land Ownership Survey of Ganges Village (Action 2A)

#### What is the move?

Per Action 2A, a major challenge in addressing active transportation infrastructure improvements and accessibility challenges in Ganges Village is the fragmented nature of jurisdictional boundaries and land ownership.

#### Why is it important?

A land ownership survey would allow the CRD to undertake a more formal assessment of the condition of the sidewalks, pathways, crosswalks, and curbs to determine legal responsibility, resource requirements and develop design specifications and maintenance standards. It would represent an important first step in addressing accessibility challenges in the village.



### 8.1.5 Big Move 5

#### Implement Fulford Ganges Road Pedestrian and Cycling Facilities from Seaview Avenue to Lower Ganges Road (Action 1C)

##### What is the move?

Per Action 1C-1 and 2C, new active transportation facilities are recommended for this corridor including protected cycling facilities on both sides, and an adaptive sidewalk on the south side. Traffic calming improvements are also recommended including a pedestrian refuge island and curb extensions, which would create a “village gateway” on the south end.

##### Why is it important?

The provision of new active transportation facilities in this area of Ganges Village will help increase pedestrian and cycling safety. It will also enhance connectivity to the larger cycling network south of Ganges Village and connect to the larger network envisioned in the village.

The Fulford Ganges Road corridor between Seaview Avenue and Lower Ganges Road serves as the southern gateway for Ganges Village and sets the tone for the increased presence of people walking and cycling within the Village. This gateway has been prioritized due to the community’s concerns around speeding on Ganges hill entering the Village and safety concerns resulting from haphazard on-street parking during summer market days.

## 8.2 Capital Planning

### 8.2.1 The Big Moves

The five “big moves” are recommended to be implemented first. The overall cost for big moves 1, 4, and 5, is anticipated to be relatively low and will depend on several factors such as overall signage requirements, the complexity of the land ownership survey, and whether the CRD acquires land for a new off-street parking location. Based on the Class D cost estimates, the total cost of Big Move 2 (Action 1A) and Big Move 5 (Action 1C) is approximately \$432,000.

The table below provides an order of magnitude (Class D) cost estimate for all of the infrastructure improvements identified in Strategy Area 1. The cost estimates are based on concept level information using unit rates for linear works and improvements. The actual costs for implementation for each project could vary and will be confirmed through additional engagement and the detailed design stage. The total level of investment for the infrastructure projects is approximately \$1,647,000 excluding GST.

Per Action 2E, 3B and 3C, vehicle parking will continue to be an important amenity within Ganges Village. The loss of on-street parking associated with active transportation facilities presents an opportunity to improve existing accessible on-street parking and increase the supply of off-street parking.

Action ID & Project Name		Class D Cost Estimate (2023 \$)
1A	Implement McPhillips Avenue pedestrian facilities	\$154,000
1B-1	Implement Rainbow Road pedestrian and cycling facilities (from Jackson Road to Lower Ganges Road)	\$397,000
1B-2	Implement Rainbow Road pedestrian and cycling facilities (from Kanaka Road to 166 Rainbow Road)	
1B-3	Implement Rainbow Road pedestrian and cycling facilities (west of Windsor Plywood)	
1C-1	Implement Fulford Ganges Road pedestrian and cycling facilities (from Seaview Avenue to Lower Ganges Road)	\$278,000
1C-2	Implement Fulford Ganges Road pedestrian and cycling facilities (from Lower Ganges Road to Purvis Lane)	\$122,000
1D-1	Implement Lower Ganges Road pedestrian and cycling facilities (from Fulford-Ganges Road to Rainbow Road)	\$568,000
1D-2	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Interim Solution)	\$128,000
1D-3	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Ultimate Solution)	\$154,000
Total		\$1,647,000

## 8.3 Operational & Maintenance Considerations

The proposed active transportation facilities within Ganges Village will be required to be maintained regularly. There are several hazards that could impact the ability for someone to use a pedestrian or cycling facility including snow, glass or debris, potholes and cracked surfaces, and leaves, for example. The CRD will need to ensure it has a maintenance program in place for the active transportation network, which could include the following:

- Sweeping and removing gravel, debris, and leaves; trimming adjacent vegetation; and adjusting bollards and other elements related to protected bike lane delineators.
- Clearing and removal of debris and snow in the fall and winter and treat and remove ice or slippery conditions.
- There are also asset management activities, which can include repairing pavement surfaces and other road surface appurtenances such as utility covers; replacing worn pavement markings, signs, and signals; mitigating locations with pooling water or drainage issues; replacing broken delineators; maintaining street and path lighting; and repairing and maintaining equipment that is used to maintain cycling facilities.





## 8.4 Action Plan

The following table outlines the 26 recommended action items under the three strategy areas:

1. Reallocate Street Space for Walking, Rolling, and Cycling within Ganges Village
2. Make Ganges Village Accessible for All Residents and Visitors
3. Encourage Sustainable Modes

For each of the recommended action items, the following have been identified:

**Timeframe:** The immediate term refers to 1-3 years; short to medium-term is 3-7 years; and long-term refers to 7-10 years. The following timeframes should be used as a guide, however, the overall prioritization of the action(s) may change over time due to shifting priorities and opportunities.

**Partners:** The CRD is responsible for most actions but collaboration with other organizations / stakeholders where applicable have been identified below.

**Implementation Approach:** There are different ways to implement an action including (1) capital project; (2) operational / maintenance; (3) technical study; (4) policy / regulatory; and (5) programming / advocacy.





Action ID & Project Name		Timeframe	Partners	Implementation Approach
Strategy Area 1: Reallocate Street Space for Walking, Rolling, and Cycling within Ganges Village				
1A	Implement McPhillips Avenue pedestrian facilities	Immediate	MOTI	Capital Project
1B-1	Implement Rainbow Road pedestrian and cycling facilities (from Jackson Road to Lower Ganges Road)	Medium-Term	MOTI	Capital Project
1B-2	Implement Rainbow Road pedestrian and cycling facilities (from Kanaka Road to 166 Rainbow Road)	Medium-Term	MOTI	Capital Project
1B-3	Implement Rainbow Road pedestrian and cycling facilities (west of Windsor Plywood)	Medium-Term	MOTI	Capital Project
1C-1	Implement Fulford Ganges Road pedestrian and cycling facilities (from Seaview Avenue to Lower Ganges Road)	Immediate	MOTI	Capital Project
1C-2	Implement Fulford Ganges Road pedestrian and cycling facilities (from Lower Ganges Road to Purvis Lane)	Medium-Term	MOTI	Capital Project
1D-1	Implement Lower Ganges Road pedestrian and cycling facilities (from Fulford-Ganges Road to Rainbow Road)	Medium-Term	MOTI	Capital Project
1D-2	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Interim Solution)	Medium-Term	MOTI	Capital Project
1D-3	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Ultimate Solution)	Long-Term	MOTI	Capital Project
1E	Adopt infrastructure design guidelines	Immediate	MOTI	Capital Project
1F	Connect to active transportation network outside of Ganges Village	Long-Term	MOTI	Capital Project

Action ID & Project Name		Timeframe	Partners	Implementation Approach
Strategy Area 2: Make Ganges Village Accessible for All Residents and Visitors				
2A	Conduct a land ownership survey of Ganges Village	Immediate	Businesses, Land Owners	Technical Study
2B	Adopt a 30 km/h posted speed limit in Ganges Village	Immediate	MOTI	Policy / Regulatory
2C	Create village gateways with traffic calming design interventions	Medium-Term	MOTI	Capital Project
2D-1	Undertake intersection review of Fulford-Ganges Road and Lower Ganges Road	Medium-Term	MOTI	Capital Project
2D-2	Undertake intersection review of Fulford-Ganges Road and Purvis Lane	Medium-Term	MOTI	Capital Project
2D-3	Undertake intersection review of Lower Ganges Road and McPhillips Avenue	Medium-Term	MOTI	Capital Project
2D-4	Undertake intersection review of Lower Ganges Road and Herford Avenue	Medium-Term	MOTI	Capital Project
2D-5	Undertake intersection review of Lower Ganges Road and Rainbow Road	Medium-Term	MOTI	Capital Project
2D-6	Undertake intersection review of Lower Ganges Road and Upper Ganges Road	Medium-Term	MOTI	Capital Project
2E	Retain and retrofit accessible parking within the Village	Immediate	MOTI	Capital Project
2F	Establish no-parking zones with physical barriers	Immediate	MOTI	Capital Project

Action ID & Project Name		Timeframe	Partners	Implementation Approach
<b>Strategy Area 3: Encourage Sustainable Modes</b>				
<b>3A</b>	Develop a bike parking retrofit program	Immediate	Businesses, Land Owners	Programming / Advocacy
<b>3B</b>	Identify opportunities to increase off-street parking	Immediate	Businesses, Land Owners	Operational / Maintenance
<b>3C</b>	Develop a parking wayfinding program	Immediate	N/A	Programming / Advocacy
<b>3D</b>	Work with BC Transit to integrate active transportation modes	Immediate	BC Transit	Programming / Advocacy

## 8.5 Funding Opportunities

As outlined in Section 8.2, the total cost for the active transportation infrastructure projects is \$1,647,000. As an unincorporated community with limited financial resources, the projected cost of all the projects will be challenging to cover for the Capital Regional District alone. Fortunately, there are many funding opportunities available that could pay for these projects over the 10-year horizon of this plan.

A summary of each funding opportunity is below.

### 8.5.1 CRD Funding

The CRD Salt Spring Island Transportation service was established in 2007 by Bylaw No. 3438 to:

- Provide a public transportation system on Salt Spring Island;
- Carry out transportation studies;
- Provide for the construction, installation, maintenance and regulation of sidewalks and bicycle paths, parking facilities, pedestrian and safety and traffic calming facilities; and
- Implement transportation demand management programs.

The service is funded through an annual tax requisition. The current Financial Plan (2023-2027) supports investments in active transportation infrastructure investments with approximately 50% of the funding being allocated to capital spending and supplemented with available grant funding. According to the 2023-2027 capital plan, a total of \$909,500 has been allocated to transportation for several improvements including pedestrian pathway improvements, maintenance and equipment—some of which could be allocated to the recommended projects in the ATNP.

### 8.5.2 BC Active Transportation Infrastructure Grant Program

The B.C. Active Transportation Infrastructure Grants Program offers two grant options for Indigenous governments and local governments, including municipalities, regional districts, and Islands Trust. Specifically, the Active Transportation Infrastructure Grant allows eligible governments to apply for a maximum of two grants if they satisfy the following criteria (based on the 2022 intake):

- Projects previously funded prior to 2022/23, or prior to 2021/22 for projects with budgets over \$1M, must be completed by application submission date.
- Project is part of an active transportation network plan or equivalent.
- Project can begin construction once provincial funding has been announced.
- Projects will be completed by March 2025 (projects under \$1 million) or by March 2026 (projects over \$1 million).
- Projects are open to the public.

The grant program typically requires that projects be “shovel-ready”. The CRD, in collaboration with its partners, could begin to action the infrastructure projects and position itself to apply for funding for the next grant intake (2023-2024), which opens September 1, 2023. The program guidelines provide the specific detail on what constitutes as a “shovel-ready” project, which includes the following:

- The Cost Estimate submitted with the Grant Application must be Class A-C and current or forecasted to proposed construction date.
- All project design work is complete.
- Community consultation is complete.

The province cost-shares to a maximum of \$500,000 per project and the CRD would be eligible for 70% of the provincial funding.

### 8.5.3 Green Municipal Funds

The Green Municipal Fund (GMF) is a program administered by the Federation of Canadian Municipalities intended to help Canadian communities expand their sustainability initiatives. Since 2000, the GMF has deployed \$900M in financing to 1,250+ sustainability initiatives and a further \$1 billion has been committed to the fund through the Federal 2019 budget.

The specific GMF initiative that is relevant to the Salt Spring context is the “Capital Project Transportation Networks Commuting Options”, which is a combined loan and grant funding program for capital projects that reduce pollution by improving transportation systems and networks. This program covers several topics including bike paths, walking and cycling networks that promote accessibility and safety, and evaluation of active transportation infrastructure, among others.



## 8.6 Next Steps

Salt Spring Island, and Ganges Village in particular, is a small community with an appetite for change. The ATNP is intended to move the community on a path toward better, safer, and more inclusive active transportation. As a smaller community with limited financial and staff resources, the CRD, and its associated partners, will need to move quickly on the low-cost, quick-build solutions identified in the ATNP. Not everything can or should be implemented tomorrow; however, there are several actions—including the McPhillips Avenue improvements and 30 km/h posted limit within the village—that can be implemented in the near term to improve active transportation for all users.

The ATNP should be a living document and not sit on the proverbial bookshelf collecting dust. It is recommended that the ATNP be revisited in 5 years (2028) to explore what has—and has not—been implemented to date and whether any of the actions in this plan need to be reprioritized.

A more walkable, bike friendly, and accessible Ganges Village will take time, and is anticipated to boost the local economy, lower the community's GHG emissions, and enhance safety for all.





Pedestrian Facilities  
(Sidewalks, Pathways)

Lifting facilities  
(for wheelchairs, strollers, etc.)



# Salt Spring Island Active Transportation Network Plan Presentation to Local Community Commission

July 18, 2023



[WATTCONSULTINGGROUP.COM](http://WATTCONSULTINGGROUP.COM)





# Agenda

1. Project Background
2. Why a Focus on Ganges?
3. Guiding Principles
4. Strategic Framework
5. The “Big Moves”
6. Capital Costs
7. Next Steps
8. Q&A





# 1. Project Background



# Project Background

## Background

- Salt Spring Island Transportation Commission directed staff to deliver an Active Transportation Plan for Salt Spring Island
- Plan funded in part from the Province of BC through the BC Active Transportation Network Planning Grant
- Presentation of plan was delayed as a result of the SSI governance changes – province has given SSI an extension in its completion dates





# Project Background

## Scope of Plan

- Focus on making Ganges a walkable community that is easily accessible by cycling
- Prioritize low-cost, quick-build, & actionable solutions recognizing limited resources
- Recognize and plan with the unique nature of SSI in mind – big city solutions not necessarily needed but must still meet all engineering and regulatory requirements





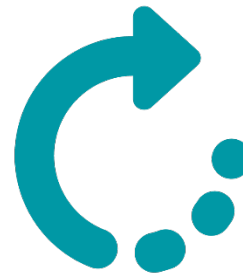
## PROJECT TIMELINE



**PHASE 1**  
Understanding



**PHASE 2**  
Listening



**PHASE 3**  
Developing



**PHASE 4**  
Finalizing

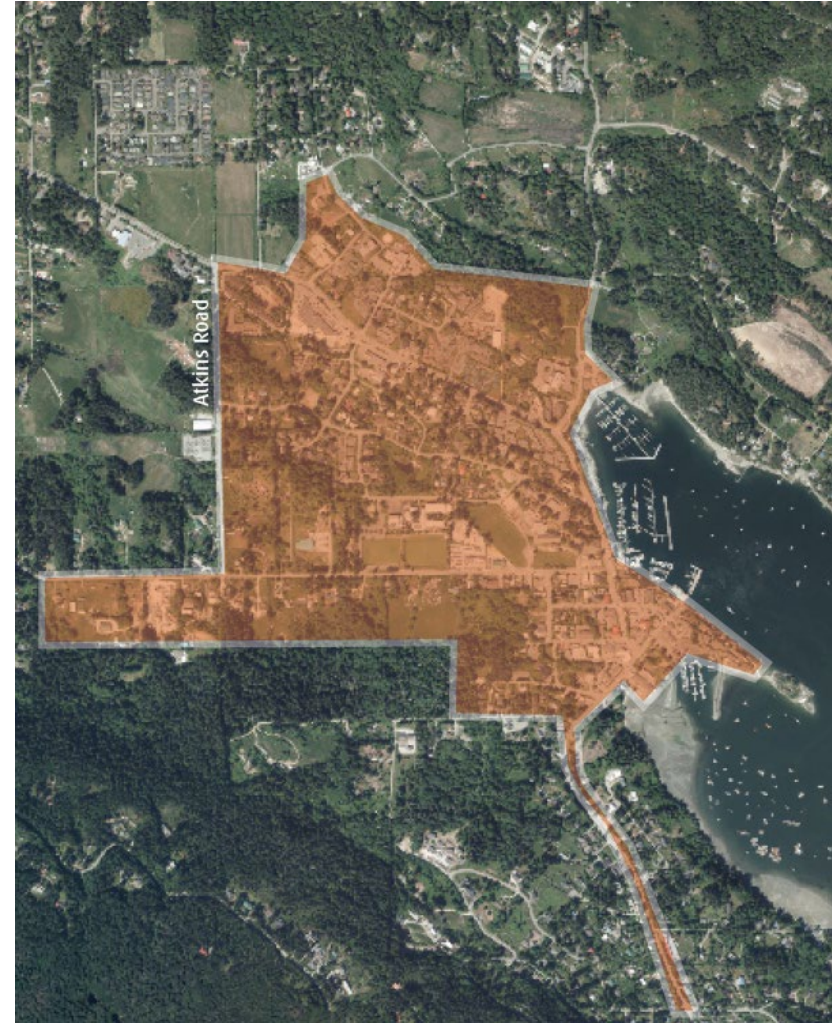


## 2. Why a Focus on Ganges?



## Ganges Focus

- Undertaken simultaneously with Salt Spring Island Cycling Safety Review
- Consistent with original scope
- The 2013 plan was silent on improvements needed in the village





# Cycling Safety Review

- MOTI study undertaken from summer 2022 to spring 2023
- Focus was island wide
- Purpose was to explore the improvements needed for cycling safety on the major roadways that connect the ferry terminals on Salt Spring including Fulford-Ganges Road, Lower Ganges Road, and Vesuvius Bay Road



# 3. Engagement Summary





# What We Heard

252

online survey #1  
respondents

155+

ideas fair  
participants



20

interactive mapping activity  
participants



14

residents provided their  
input through an ASK Salt  
Spring conversation



13

in-person stakeholder  
workshop participants

82

online survey #2  
respondents



# 4. Guiding Principles



# Guiding Principles

1. Low-cost, Quick-build, & Actionable Solutions
2. A Pedestrian Friendly Village
3. Integration & Connectivity
4. A More Accessible Ganges
5. Active Transportation Solutions are Climate Solutions



# 5. Strategic Framework





# Strategic Framework

## Strategy Area 1 Reallocate Street Space for Walking, Rolling, and Cycling

### Action 1A:

Implement McPhillips Avenue pedestrian facilities

### Action 1B:

Implement Rainbow Road pedestrian facilities

### Action 1C:

Implement Fulford Ganges Road pedestrian and cycling facilities

### Action 1D:

Implement Lower Ganges Road pedestrian and cycling facilities

### Action 1E:

Adopt infrastructure design guidelines

### Action 1F:

Connect to active transportation network outside of Ganges Village

## Strategy Area 2 Make Ganges Village Accessible for all Residents and Visitors

### Action 2A:

Conduct a land ownership survey of the Ganges Village

### Action 2B:

Adopt a 30 km/h posted speed limit in Ganges Village

### Action 2C:

Create village gateways with traffic calming design interventions

### Action 2D:

Undertake reviews of intersections within Ganges Village

### Action 2E:

Retain accessible parking within the Village

### Action 2F:

Establish no-parking zones with physical barriers where necessary

## Strategy Area 3 Encourage Sustainable Modes

### Action 3A:

Develop a bike parking retrofit program

### Action 3B:

Identify opportunities to increase off-street parking

### Action 3C:

Develop a parking wayfinding program

### Action 3D:

Work with BC Transit to integrate active transportation modes

Big Move



# The Big Moves

1. **Low-cost** | In alignment with the first guiding principle of the ATNP, a big move should be relatively low cost recognizing the limited financial resources for infrastructure on the island.
2. **Actionable** | In alignment with the first guiding principle of the ATNP, a big move should be actionable, meaning that it requires minimal staff time and strong support from key stakeholders such as MOTI to make implementation seamless.
3. **High Impact** | high impact refers to increasing the number of people walking, cycling, and rolling while improving safety and accessibility for the most vulnerable road users.



# Big Move #1 – 30 km/h Posted Speed Limit

- High vehicle speeds a top concern heard in engagement
- Aligns with recommendation in the *Cycling Safety Review*
- Aligns with guiding principles of “creating a more pedestrian friendly village”





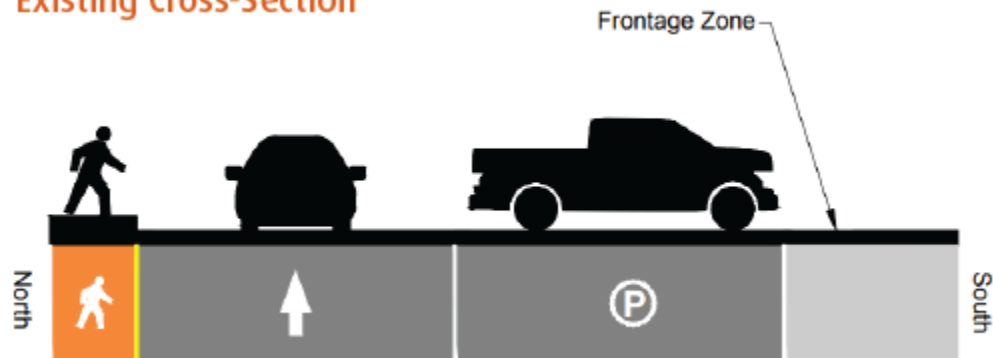






## Big Move #2 – McPhillips Avenue (Option 1)

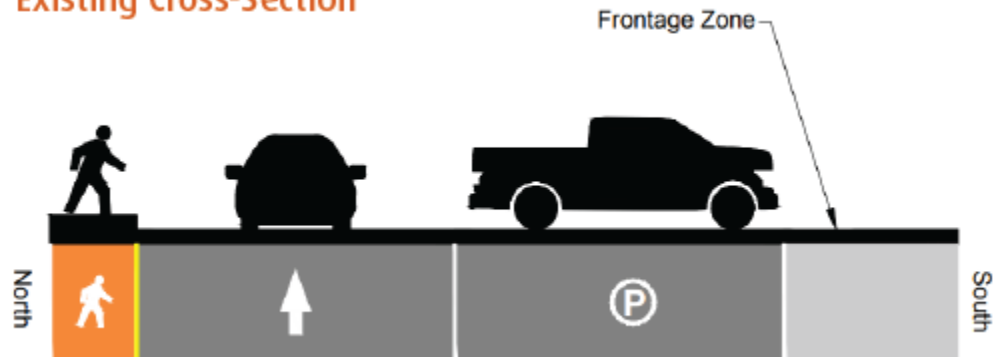
Existing Cross-Section



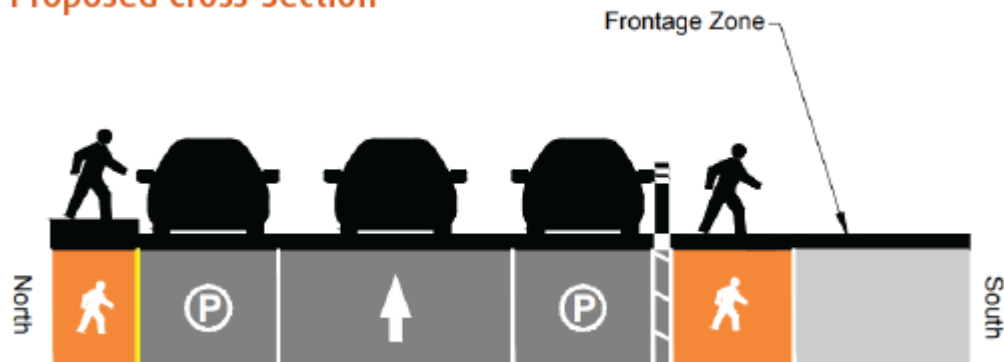


# Big Move #2 – McPhillips Avenue (Option 1)

Existing Cross-Section



Proposed Cross-Section





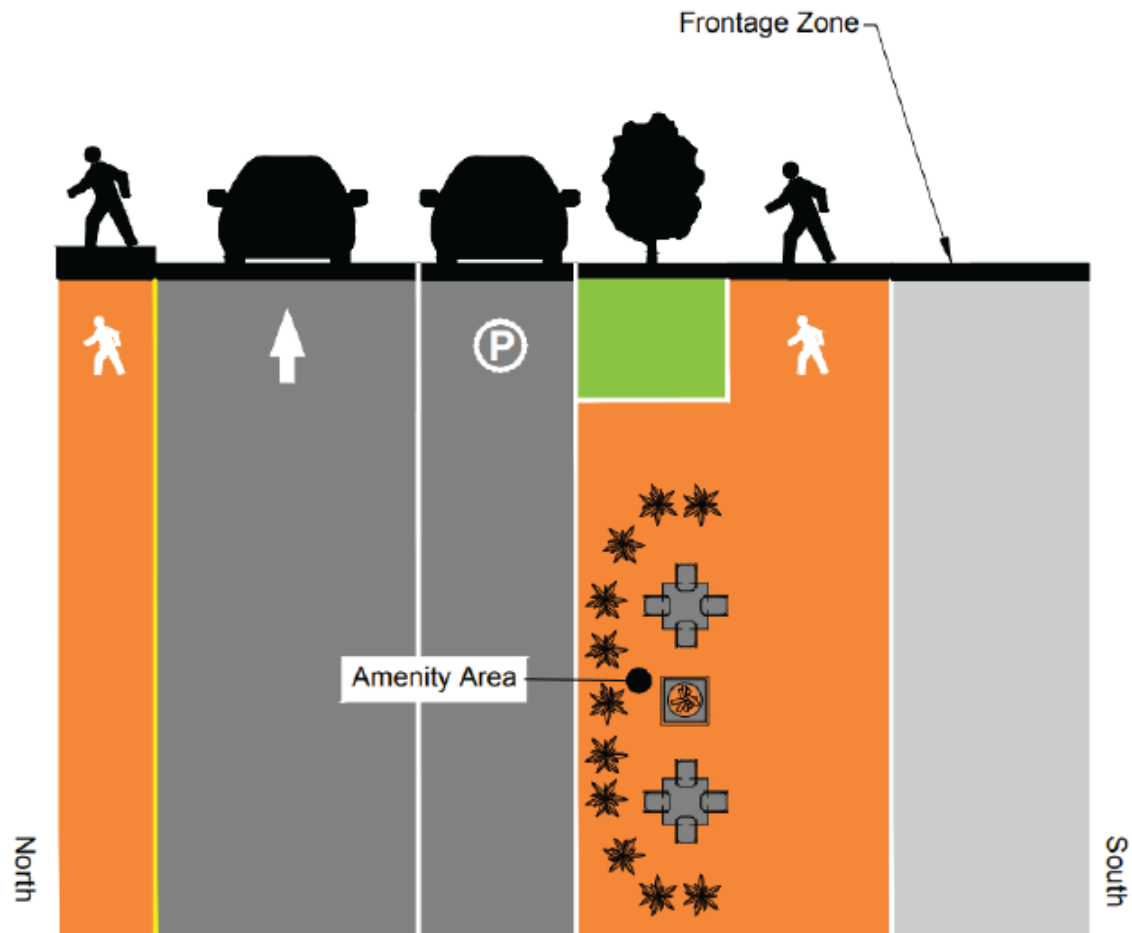
## Big Move #2 – McPhillips Avenue (Option 1)





## Big Move #2 – McPhillips Avenue (Option 2)

### Proposed Cross-Section for Seasonal Activation







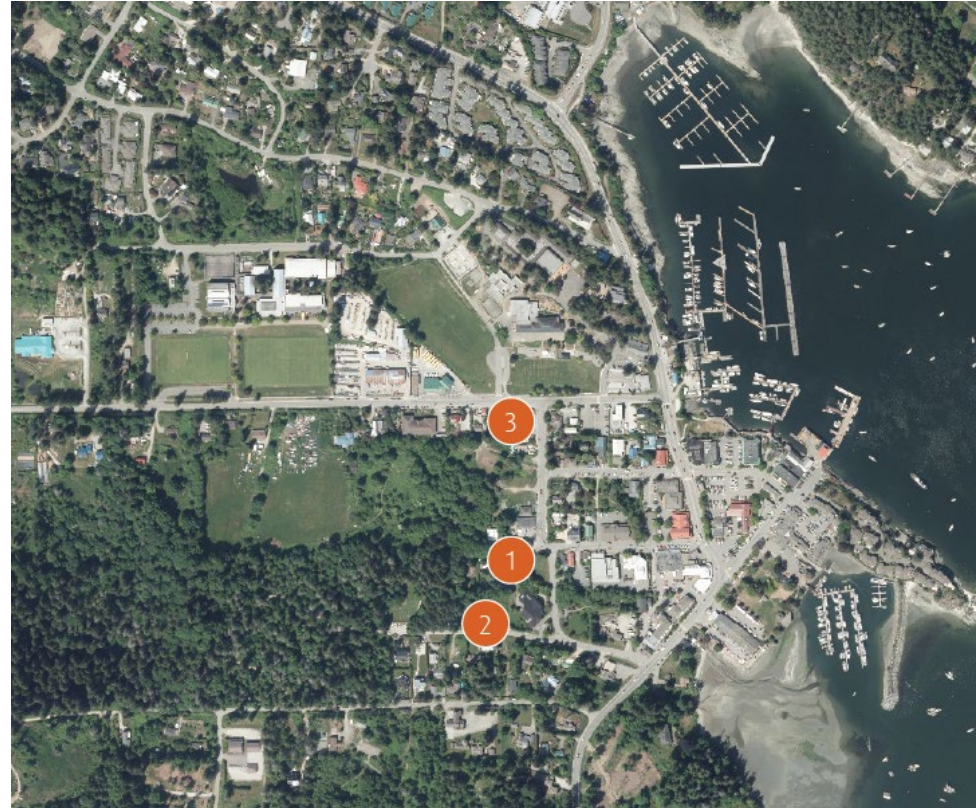
## Big Move #2 – McPhillips Avenue (Option 2)





## Big Move #3 – Increasing Off-Street Parking Options

- To offset loss in on-street parking to accommodate new facilities, expand opportunities for off-street parking
- Three potential locations:
  1. Row of stalls at ArtSpring
  2. Off-street lot west of ArtSpring
  3. Private lot along Rainbow Rd near Jackson Ave







## Big Move #4 – Land Ownership Survey

- Jurisdictional boundaries and ownership is fragmented
- Land ownership survey of the village to identify ownership of properties (MOTI vs. private land vs. commercial entities)
- A survey would allow CRD to undertake assessment of active transportation facilities and develop design specifications and maintenance standards





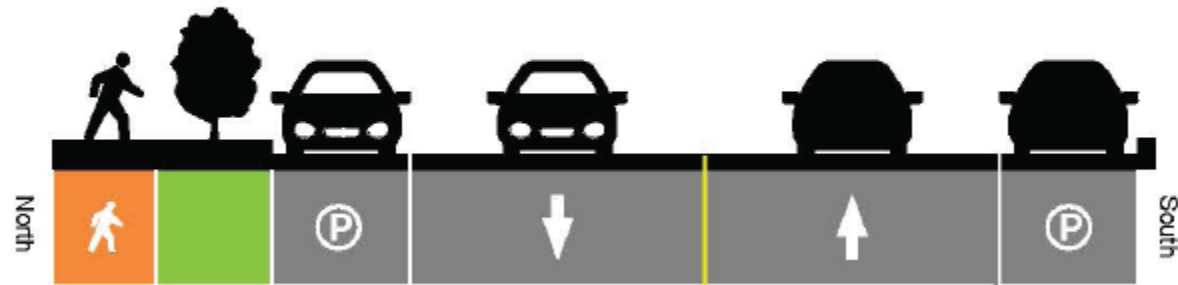






# Big Move #5 – Fulford-Ganges

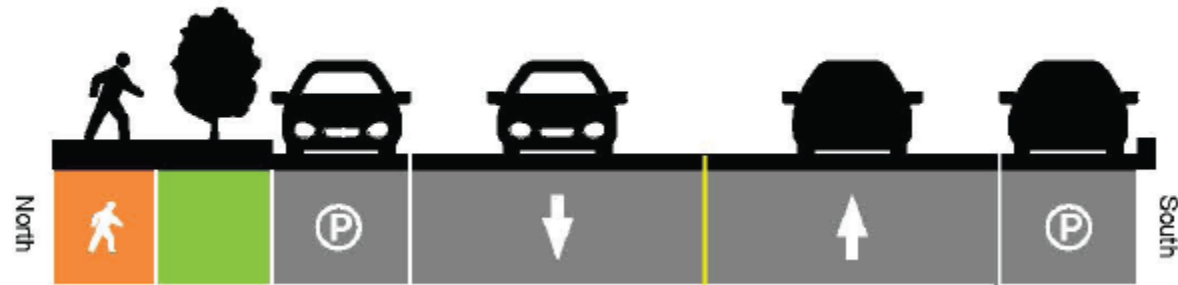
Existing Cross-Section



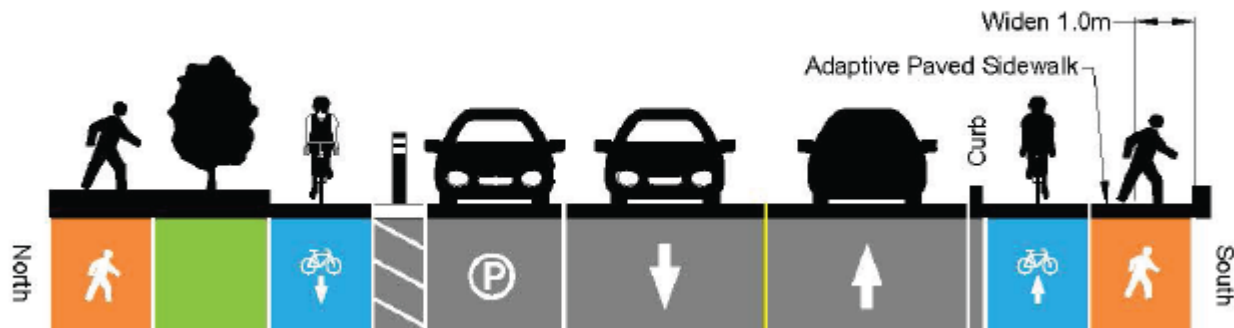


# Big Move #5 – Fulford-Ganges

Existing Cross-Section



Proposed Cross-Section





# Big Move #5 – Fulford-Ganges





## Big Move #5 – Fulford-Ganges





# 6. Capital Planning



## Capital Planning

Action ID & Project Name		Class D Cost Estimate (2023 \$)
1A	Implement McPhillips Avenue pedestrian facilities	\$154,000
1B-1	Implement Rainbow Road pedestrian and cycling facilities (from Jackson Road to Lower Ganges Road)	\$397,000
1B-2	Implement Rainbow Road pedestrian and cycling facilities (from Kanaka Road to 166 Rainbow Road)	
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1D-3	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Ultimate Solution)	\$154,000
Total		\$1,647,000

# 7. Next Steps



## Next Steps

- Focus on the easy, low-cost, actionable “quick wins”:
  - McPhillips Ave
  - 30 km/h posted speed limit in the village
- Revisit the plan in 5 years!
- Do not forget about the “softer” infrastructure (e.g., bike parking, wayfinding, transit integration)

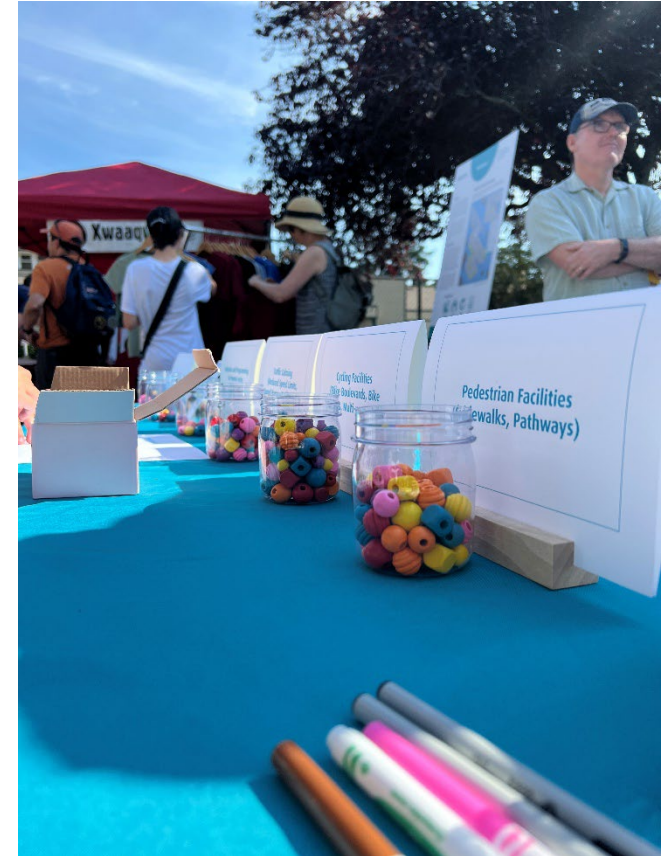






## Next Steps

- Opportunity to design both McPhillips Avenue and Fulford-Ganges
- Additional capital funding would be required for conceptual designs (30% design) to obtain provincial funding through the BC Active Transportation Infrastructure Grants Program





Thank you!

**REPORT TO SALT SPRING ISLAND LOCAL COMMUNITY COMMISSION  
MEETING OF TUESDAY, JULY 18, 2023**

**SUBJECT      MERCHANT MEWS PATHWAY DESIGN – ADDITIONAL FUNDING**

**ISSUE SUMMARY**

Additional funding is required to complete the design of the Merchant Mews Pathway.

**BACKGROUND**

The Capital Regional District (CRD) contracted with a local Salt Spring Island not for profit organization, Island Pathways Society, for the investigation, design and construction estimate of a 300 m long pathway from the retail and commercial area known as Merchant Mews to the intersection of Upper Ganges Road and Leisure Lane along the west side of Upper Ganges Road. The project has been contemplated since 2021 with IP doing some preliminary work starting at that time. The CRD initiated a capital project in April of 2022.

The initial budget for CRD project management was \$2,000. The current CRD project management spending is at \$15,800 due to the following factors:

- Excessive amount of time spent by CRD project management supporting and guiding Island Pathways Society to produce the defined deliverables. This time, and attendant cost, is far more than what would normally be spent using an industry design consultant. To put this in perspective, the Project Manager for this project has ~ 600 e-mails in the project files for what should be a small, simple project.
- To illustrate what this design project might have cost, the design of a much more complicated and longer (1.1 km) pathway from Booth Canal Road to Vesuvius Bay Road cost approximately \$21,500 (in 2019) carried out by a professional engineering consulting company with far less involvement from CRD engineering staff.
- IP drawings did not meet Ministry of Transportation and Infrastructure standards which caused re-work as well as extra CRD PM time for additional meetings and extra correspondence.
- Considerable CRD project management time was spent applying for a BC Active Transportation grant to fund construction of the pathway which was denied because of drawings not meeting standards.
- The BC Active Transportation grant will need to be applied for again and it is hoped that much of the application documentation and information assembled for the failed submission can be re-used.

Budget amounts, along with costs to date and costs to complete are shown in the table below.

	<b>Approved budget</b>	<b>Costs to date</b>	<b>Estimated future costs to complete</b>	<b>Revised budget</b>	<b>Additional funding required</b>
Design Costs	18,000	-	16,600	16,600	- 1,400
Project Management	2,000	15,800	2,000	17,800	15,800
Contingency	-	-	2,000	2,000	2,000
<b>Total</b>	<b>20,000</b>	<b>15,800</b>	<b>20,600</b>	<b>36,400</b>	<b>16,400</b>

## **ALTERNATIVES**

### *Alternative 1*

The Salt Spring Island Local Community Commission recommends to the Capital Regional District Board that the Salt Spring Island Transportation Service 2023 Capital Plan be amended to increase the budget for the Merchant Mews project by \$16,400 funded from the Capital Reserve Fund.

### *Alternative 2*

That this report be referred back to staff for additional information.

## **IMPLICATIONS**

### *Alternative 1*

This additional funding will provide for completion of the design of the pathway and allow re-submission of the application to the Ministry of Transportation and Infrastructure for a Permit to Construct and License of Occupation.

### *Alternative 2*

A request for further information will further delay the project.

## **CONCLUSION**

Additional funding is required to complete the design of the Merchant Mews Pathway. Fortunately, adequate funds are available in the Service's Capital Reserve Fund so that there are no further delays to the completion of the design and initiation of construction.

## **RECOMMENDATION**

The Salt Spring Island Local Community Commission recommends to the Capital Regional District Board that the Salt Spring Island Transportation Service 2023 Capital Plan be amended to increase the budget for the Merchant Mews project by \$16,400 funded from the Capital Reserve Fund.

Submitted by:	Dean Olafson, P. Eng., MBA, Manager of Engineering, SSI Electoral Area
Concurrence:	Karla Campbell, MBA, BPA, Senior Manager, SSI Electoral Area
Concurrence:	Lia Xu, M. Sc., CPA, CGA, Finance Manager, Local Services
Concurrence:	Ted Robbins, B. Sc., C. Tech., Chief Administrative Officer





Making a difference...together

## **REPORT TO LOCAL COMMUNITY COMMISSION MEETING OF TUESDAY, JULY 18, 2023**

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**SUBJECT**      **Portlock Park Site Master Plan – Three Conceptual Designs**

**ISSUE**

To review three conceptual designs for further community consultation on the Portlock Park Site Master Plan.

**BACKGROUND**

Portlock Park was transferred to the Capital Regional District (CRD) in 1976. At that time there was a small office for parks and recreation administration purposes combined with a concession area, maintenance room, sports equipment storage area, and two public washrooms.

In 1992, the building was renovated to convert the office, concession area, maintenance room equipment storage into offices. In addition, an outdoor pool with changing rooms, washrooms and showers were added. A 24 x 40 ft. portable was added to the premises during the mid-1990's and was used as a community meeting room and recreational programming space until it was closed in September 2014 due to contamination. The contamination was mitigated in 2015 and the portable was re-opened but it is close to the end of its useful life. In 2006, the outdoor pool was decommissioned, and the indoor changing rooms were no longer required and are currently used for storage.

In 2012, the CRD set up the electoral area administration to manage and operate, transportation, economic development, water and sewer utilities, and capital infrastructure on island which operated out of the Portlock office building. The administration office was relocated to Ganges in 2017 and the Portlock office is currently used by park maintenance, bylaw staff and sports groups.

Portlock Parks is located in the ALR with approval for non-farm use granted in 1992 (Legacy Application #26614, Resolution 353/1992). Additions to the site after 1992 and any future development would require an updated application to the Agricultural Land Commission.

The development of Portlock Park site master plan was identified as a priority in the 2019 Salt Spring Island Parks and Recreation Strategic Plan to enhance community use, develop additional ball fields and replace aging infrastructure in current or new locations. Relocating some of the park amenities from Portlock Park to the Rainbow Recreation Centre site will allow for more playing fields, specifically a senior sized baseball field for youth and adults on the Portlock site. PARC has allocated \$30,000 from capital reserves to begin the preliminary assessments, community consultation and development of the site master plan.

The first round of public engagement has finished with a public survey that was available from February 17, 2023 to March 6, 2023. In addition to the public survey, initial meetings with stakeholders, staff and the former Salt Spring Island Parks and Recreation Commission are now complete. Survey results (Attachment 1) and notes from stakeholder workshops have been compiled into three conceptual designs for consideration (Attachment 2).

The next steps in the process will include consultation with the Salt Spring Island Local Community Commission (LCC) to finalize three conceptual designs to be used in a second public survey. Community feedback will be incorporated into one final design for LCC consideration.

### **ALTERNATIVE**

That the Salt Spring Island Local Community Commission (LCC) recommends that

#### **Alternative 1**

The three Landscape Conceptual designs developed by LADR Landscape Architects dated June 23, 2023 be used for further community consultation.

#### **Alternative 2**

The three Landscape Conceptual designs developed by LADR Landscape Architects dated June 23, 2023 be used as amended for further community consultation.

#### **Alternative 3**

The report be referred back to staff for additional information.

### **CONCLUSION**

The development of Portlock Park Site Master Plan was identified as a priority in the 2019 Salt Spring Island Parks and Recreation Strategic Plan. The first round of public engagement was completed with a public survey, staff and stakeholder workshops. The feedback has been incorporated into three conceptual design drafts.

This report has been provided for LCC to review the draft conceptual designs that will be used for further community consultation including a public survey, staff and stakeholder workshops.

### **RECOMMENDATION**

That the Salt Spring Island Local Community Commission (LCC) recommends that the three Landscape Conceptual designs developed by LADR Landscape Architects dated June 23, 2023 be used for further community consultation.

Submitted by:	Dan Ovington, Manager, Parks and Recreation
Concurrence:	Karla Campbell, Senior Manager, SSI Electoral Area

DO: sc

Attachments:

Attachment 1 - Portlock Park Site Master Plan Landscape Conceptual Plans 1 - 3



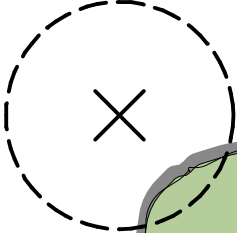
LEGEND - OPTION 1

- 1. New Senior Ballfield With Grass Infield - 300' Outfield, 20' Backstop
- 2. New Seasonal Soccer Field - 196' x 320' play area
- 3. New Skinned Junior Ballfield - 200' Outfield, 10' Backstop
- 4. Expanded Storage Shed - 75' x 20'
- 5. New Pickleball Courts (6) - 30' x 60' play area each
- 6. New 1.5m Wide Walking/Jogging Trail - 0.4km
- 7. New 3m Wide Walking/Jogging Track - 0.3km
- 8. Existing Portable Removed & Staff Parking Added With EV Stall
- 9. New Swale / Raingarden at Base of Embankment
- 10. Existing Playgrounds
- 11. Existing Multi-Purpose Court Relocated - Approx. 53.5' x 86'
- 12. New Batting Cage - 70' x 14' each
- 13. New Practice Soccer Field - 49' x 80' play area
- 14. New Practice Soccer Field - 98' x 160' play area
- 15. Expanded Paved Parking Lot With Turnaround Loop: 91 Stalls
- 16. New Storage Shed for Soccer/Baseball - 10' x 20'
- 17. Existing Water Storage Tank Relocated - 10' x 15'

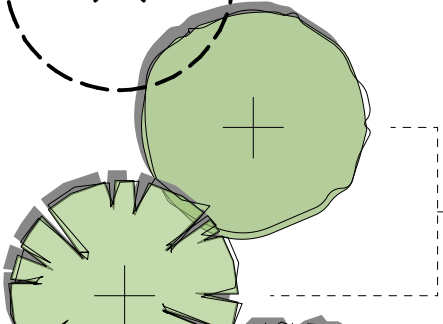
NOTES

- 1. Tennis courts relocated to Rainbow Recreation Centre
- 2. Existing park office to include upgraded concession, additional washrooms, change room, and showers

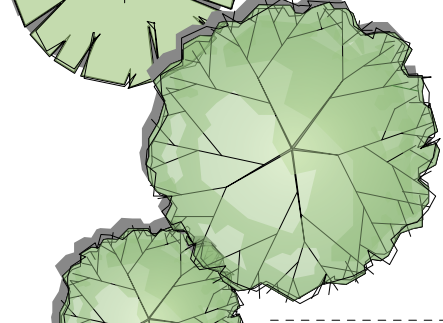
TREE LEGEND



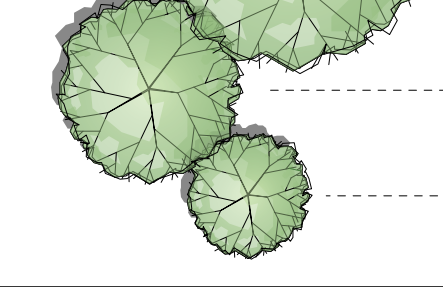
EXISTING TREE (REMOVED)



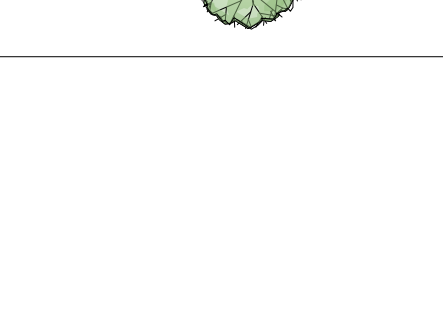
EXISTING TREES (RETAINED)



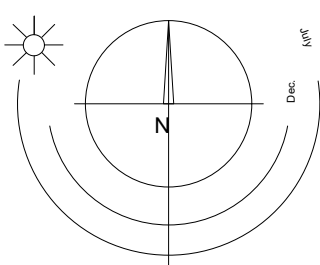
PROPOSED TREE (LG CANOPY)



PROPOSED TREE (MED CANOPY)



PROPOSED TREE (SM CANOPY)



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C	Jul 06-23	Issued for Client Review
B	Jul 04-23	Issued for Client Review
A	Jun 23-23	Issued for Client Review

REVISIONS



LADR LANDSCAPE ARCHITECTS

#3-864 Queens Ave. Victoria B.C. V8T 1M5  
Phone: (250) 598-0105

PROJECT

Portlock Park Master Plan  
145 Vesuvius Bay Rd  
Salt Spring Island

TITLE

Conceptual Master Plan  
Option 1

SCALE1:500

DRAWN NR/CW  
CHECKED CW

PROJECT No. 2304

DATEJun 9-23

1 of 3  
SHEET



LEGEND - OPTION 2

- 1. New Senior Ballfield With Grass Infield - 300' Outfield, 20' Backstop
- 2. New Seasonal Soccer Field - 196' x 320' play area
- 3. New Senior Ballfield With Grass Infield - 300' Outfield, 15' Backstop
- 4. New Practice Soccer Fields (2) - 98' x 160' play area each
- 5. New Pickleball Courts (4) - 30' x 60' play area each
- 6. New 4m Wide Walking/Jogging Perimeter Trail - 0.6km
- 7. Expanded Paved Parking Lot With Turnaround Loop: 58 Stalls
- 8. Existing Portable Removed & Staff Parking Added With EV Stall
- 9. New Swale / Raingarden at Base of Embankment
- 10. Existing Playgrounds
- 11. Existing Multi-Purpose Court Relocated/Replaced - Approx. 53.5' x 86'
- 12. New Batting Cage - 70' x 14'
- 13. Relocated and Expanded Storage Shed - 75' x 20'
- 14. Existing Water Storage Tank Relocated - 10' x 15'
- 15. New Storage Shed for Soccer/Baseball - 10' x 20'

NOTES

- 1. Tennis courts relocated to Rainbow Recreation Centre
- 2. Existing park office to include upgraded concession, additional washrooms, change room, and showers

TREE LEGEND

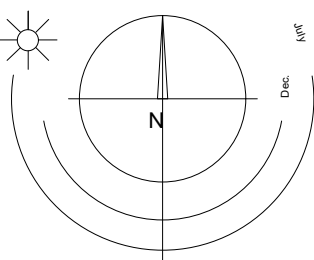
EXISTING TREE (REMOVED)

EXISTING TREES (RETAINED)

PROPOSED TREE (LG CANOPY)

PROPOSED TREE (MED CANOPY)

PROPOSED TREE (SM CANOPY)



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REVISIONS

**LADR LANDSCAPE ARCHITECTS**

#3-864 Queens Ave. Victoria B.C. V8T 1M5  
Phone: (250) 598-0105

PROJECT  
**Portlock Park Master Plan**  
**145 Vesuvius Bay Rd**  
**Salt Spring Island**

TITLE  
**Landscape Master Plan**  
**Option 2**

SCALE  
**1:500**

DRAWN **NR/CW**  
CHECKED **CW**

PROJECT No. **2304**

DATE  
**Jun 9-23**

**2** of **3**  
SHEET



LEGEND - OPTION 3

- 1. Existing Portable Removed & Staff Parking Added With EV Stall
- 2. Existing Track Repositioned - Approx. 0.4km
- 3. Existing Soccer Field Repositioned - Approx. 196' x 320'
- 4. New Skinned Junior Ballfield - 200' Outfield
- 5. New Pickle Ball Courts (6) - 30' x 60' play area each
- 6. Expanded Paved Parking With Turnaround Loop: 67 Stalls
- 7. Existing Practice Soccer Field - Approx. 128' x 192'
- 8. Existing Junior Ballfield Repositioned - 200' Outfield
- 9. Existing Practice Soccer Fields Repositioned (3) - Approx. 61' x 116' each
- 10. Existing Batting Cage Relocated - 75' x 15'
- 11. New Swale / Raingarden at Base of Embankment
- 12. Existing Playgrounds
- 13. Existing Multi-Purpose Court Relocated/Replaced - Approx. 53.5' x 86'
- 14. Expanded Storage Shed - 75' x 20'
- 15. Existing Water Storage Tank Relocated - 10' x 15'
- 16. New Storage Shed for Soccer/Baseball - 10' x 20'

NOTES

- 1. Tennis courts relocated to Rainbow Recreation Centre
- 2. Existing park office to include upgraded concession, additional washrooms, change room, and showers

TREE LEGEND

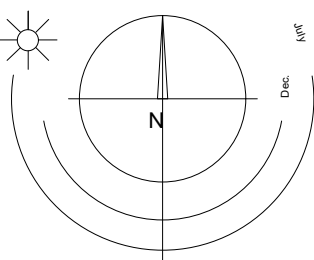
EXISTING TREE (REMOVED)

EXISTING TREES (RETAINED)

PROPOSED TREE (LG CANOPY)

PROPOSED TREE (MED CANOPY)

PROPOSED TREE (SM CANOPY)



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REVISIONS

LADR LANDSCAPE ARCHITECTS

#3-864 Queens Ave. Victoria B.C. V8T 1M5  
Phone: (250) 598-0105

PROJECT

Portlock Park Master Plan  
145 Vesuvius Bay Rd  
Salt Spring Island

TITLE

Conceptual Master Plan  
Option 3

SCALE 1:500

DRAWN NR/CW  
CHECKED CW

PROJECT No. 2304

DATE Jun 9-23

3 of 3  
SHEET



## Salt Spring Island Administration Project Tracker

Report No. 11, 2023 | April - June



Project	Description	Status	%	Comments	Timing
<b>Priority Area: Drinking Water</b>					
Fulford Water Main Replacement	Replace a section of water main that crosses Weston Creek, that has become exposed due to erosion and later failed during a storm event in November 2021.	In-Progress	70	Design finalized for permanent repair and approved by Island Health and MoTI. Construction contract awarded. Mobilization weather dependent.	Q2 2023
Fulford Water Main Distribution System Replacement	Investigation, analysis, design and strategy for replacement of asbestos/concrete sections of Fulford water main distribution system.	In-Progress	50	Consultant contract awarded Q4 2022. Draft Technical Report complete. Soliciting comments from Commission.	Q2 2023
Beddis Intake Construction	Construct and install intake screen and pipe improvements	In-Progress	10	Seeking regulatory approval prior to procurement. Procuring riparian report.	Q3 2023
Highland Fernwood Intake Construction	Construct and install intake screen and pipe improvements	In-Progress	10	Seeking regulatory approval prior to procurement. Procuring riparian report.	Q3 2023
Highland/Fernwood - DAF Float Waste Pump Design and Construct	Design and construct waste pump.	On-Hold	50	Design complete. Delayed to 2023 due to insufficient funds available for construction in 2022.	Q4 2023
Highland (Legacy Service Area)	Highland Upper Reservoir Repair.	In-Progress	50	Option analysis complete. Staff presented the preferred option and budget to the Commission for approval. Detailed design to proceed.	Q3 2023
Highland / Fernwood Back Up Power	Design back up power system for Highland /Fernwood water system.	In-Progress	95	Engineering and design essentially complete.	Q1 2023
Fulford Water System Connection	Request to include a proposed development connected to the Fulford System	In-Progress	60	Draft impact study reviewed and feedback provided.	Q2 2023
Cedars of Tuam	New well and water treatment plant designs and construction; and water conservation plan.	On-Hold	10	Federal/provincial Investing in Canada Infrastructure Program grant application submitted February 2022. Grant application unsuccessful. Staff to develop alternatives.	Q2 2023
Cedar Lane	Manganese treatment system engineering designs - Phase 1.	In-Progress	95	Engineering and design complete. Staff preparing report on the next steps to advance the infrastructure improvement and ratepayer consultation.	Q1 2023
Dam Safety	Review weir and water licenses; and CRD responsibilities	In-Progress	5	Consultation with province on inspection and reporting requirements.	Q2 2023
Mt. Erskine water system	Mt. Erskine water conversion of community water system	In-Progress	15	Sep/22 grant submitted for an Infrastructure Planning grant for Mount Erskine Water Society to assess the water society's infrastructure, operations, and financial position. Grant awarded. Procuring consultant.	Q2 2023
<b>Priority Area: Sewer/Liquid Waste</b>					
Ganges WWTP Storage	Concept Design for Storage at Ganges WWTP.	In-Progress	75	Currently reviewing preliminary design, analysis and cost estimates, as well as evaluating various options to optimize the construction of the storage/work area building. Coordinating with Aeration project.	Q2 2023
Ganges WWTP Aeration System Design	Design for upgrade to Ganges WWTP aeration system.	In-Progress	20	Consultant contract awarded Q4 2022.	Q2 2023
Ganges WWTP VFD Installation	VFD installation for the EQ Tank at the Ganges WWTP.	In-Progress	5	Project initiated. Included in Aeration System.	Q3 2023
Maliview WWTP Upgrade Detailed Design	Detailed design and cost estimation for upgrading of Maliview WWTP	In-Progress	25	Design underway. Major equipment proposals received and being evaluated. Reissued RFP for MBBR and MBR options.	Q4 2023
Maliview Electoral Assent for Borrowing	Electoral assent for borrowing of funds for the upcoming Maliview WWTP Upgrading Project	On-Hold	20	Grant awarded in the amount of \$1.98M. Staff hosted public open house/consultation March 2021. On hold until detailed designs and cost estimates are completed to determine amount of borrowing.	Q4 2023
Composting Facility	Community based composting facility and service agreement	In-Progress	40	Executing service planning and procurement of capital works to operationalize composting facility.	Q1 2024
Liquid Waste Treatment Upgrades	Phase 1: Options for treatment and disposal	In-Progress	10	Conduct an options analysis for treatment and disposal options for the Burgoyne Bay Liquid Waste Facility. Consultant contract awarded.	Q2 2023
Maliview Inflow and Infiltration Program	Investigation of sewer collection system to determine inflow and infiltration points and strategy for repair.	In-Progress	5	Project initiated.	Q4 2023

## Salt Spring Island Administration Project Tracker

Report No. 11, 2023 | April - June



Project	Description	Status	%	Comments	Timing
Ganges WWTP Membrane Lifting Brackets	Replace MBR membrane lifting brackets.	In-Progress	10	Equipment purchased. Operations to install.	Q2 2023
Liquid Waste Burgoyne Grit Chamber	Design and install Grit Chamber to reduce maintenance on pumps and improve process.	In-Progress	10	Project initiated. Design under way with Operations.	Q2 2023
<b>Priority Area: Transportation</b>					
Merchants' Mews Pathway Design	Design a separate pathway along Upper Ganges Rd from Leisure Lane to Merchants Mews	In-Progress	80	Detailed design prepared by Island Pathways; BC Active Transportation grant submitted Oct/22. Not awarded the grant. Drawings were not up to the standard to be considered detailed design drawings and did not provide enough information for MoTI to properly review the project. Staff to prepare a request for additional funds to revise drawings for permit approval.	Q2 2023
Rainbow Road Pathway Design	Design an asphalt sidewalk along Rainbow Road from 167 to Lower Ganges Road	In-Progress	5	Preparing Project Charter to define project scope.	Q3 2023
Swanson Road Pathway Design	Design a pathway along Swanson Road from Atkins to Leisure Lane.	In-Progress	5	Preparing Project Charter to define project scope.	Q3 2023
Bus Shelter	Design and construction for West Horel bus shelter.	In-Progress	20	Survey complete. Dec/22 application submitted through BC Transit's Bus Shelter Program. Will apply to MoTI's Minor Betterments program for funding.	Q3 2023
Bus Shelter	Design and construction of art-inspired bus shelter at Mobrae Avenue and Vesuvius Bay Road.	Issue has arisen	5	Design/build contract being developed with local contractor. Contractor delayed project from 2021 to 2023. Ongoing contractor delays still unresolved. Contractor to resolve issues by end of April 2023; or staff will seek alternative course of action to procure a bus shelter.	Q2 2023
Speed Reader Boards	Costing and installing speed reader boards at speed transition zones	In-Progress	25	Three of five locations approved by MoTI. Speed Reader Boards purchased. Support structure being designed.	Q2 2023
Ganges Village Transportation Plan	Develop an Active Transportation Network Plan for the Ganges area Village to guide active transportation infrastructure with a focus on walkability and connectivity across the island.	In-Progress	80	Project initiated Mar/22 for a consultant to review and identify existing infrastructure gaps and recommendations for investments. October/22: survey and online engagement process initiated to identify active transportation networks gaps and priorities for the future of active transportation on the island. Nov/22 Baseline Conditions report presented to Transportation Commission. Stakeholder workshop scheduled for Feb. 7/23; and second online survey to launch following the workshop. Report being finalized.	Q2 2023
Transit Bus Depot	Concept designs for a transit bus dept including EV chargers	see Parks Works Yard		Please see Parks Works Yard project under Park Land section for updates on a shared facility to accommodate park maintenance facility and bus depot including EV chargers for both buses and CRD vehicles	

### Priority Area: Economic Development

Economic Action Plan	Facilitate engagement on a proposed future state for economic development and sustainability on SSI	Complete	100	Contract awarded to design, facilitate, manage and report on a cross-sector stakeholder engagement process to inform the development of an Economic Action Plan (EAP). The project is an economic capacity-building strategy to strengthen existing businesses, attract green and clean new businesses, and encourage collaboration among the sectors to achieve a sustainable island economy. Community forum is scheduled for Mar 7/23. Report finalized and posted to	Q4 2022
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## Salt Spring Island Administration Project Tracker

Report No. 11, 2023 | April - June



Project	Description	Status	%	Comments	Timing
Data Hub	Salt Spring Island specific economic, social, environmental data	On-Hold	5	Work with community stakeholders to compile a community profile and socio-economic evidenced based hub of SSI sectors, jobs, businesses, employees, for economic education and building capacity. On-hold subject to capacity resource requirements.	Q4 2022
Grants Clearing House	Develop a grant information hub on <a href="https://opportunitiesaltspring.ca">https://opportunitiesaltspring.ca</a>	On-Hold	20	Research pending for compiling resources for community economic project funding and for identifying 'shovel-ready' projects. On-hold subject to capacity resource requirements.	Q3 2022
Opportunity SS Website and Info Hub	Refresh <a href="https://opportunitiesaltspring.ca/">https://opportunitiesaltspring.ca/</a> and frame social media plan for posting blog. Update content outline and refresh focus.	On-Hold	25	Review and update web content for refresh; create feature article blog posts. Digital media content specialist contracted to create engaging socio-economic content for website. On-hold subject to capacity resource requirements.	
<b>Priority Area: Parks and Recreation</b>					
<b>Park Land:</b>					
Ganges Harbour Walk	A steering committee has been established to secure right of ways, develop designs and consult with the community to build the Ganges Harbour walk.	In-Progress	10	Application submitted to the Province to secure foreshore right of way within existing alignment. Province requested detailed designs be submitted. RFP has been posted to complete consultation with upland owners, First Nations and the community to inform and complete detailed designs.	Q4 2023
Parks Works Yard	Maintenance facility designs and costing	In-Progress	5	Developing conceptual and detailed designs to move ahead with approvals. Designs will be coordinated with SSI transit O&M facility planning project for EV bus charging infrastructure and storage.	Q3 2023
Parkland Acquisition - Playing Fields	Purchase additional parkland for the purpose of playing fields	Issue has arisen	0	On hold until suitable land outside of ALR become available	
Parkland Acquisition- Mt Maxwell	Purchase of additional parkland on Mt. Maxwell for recreation	Complete	100	Fundraising campaign was successful. Purchase-Sell Agreement completed. A management plan will need to be developed to inform community uses.	Q2 2023
Portlock Park Master Plan	Develop a master plan to reconfigure the park with the replacement of failing infrastructure	In-Progress	20	Project Charter has been approved by PARC. Consultation has begun with first community survey now complete. Three conceptual designs are being drafted for further consultation	Q4 2023
Hydrofield - Field Upgrades	Proposed enhancement project to upgrade fields with two minor baseball and a soccer practice	In-Progress	5	Conceptual designs and cost estimates have been obtained. CRD is working with SD64 to formalize a joint use agreement.	Q4 2023
<b>Community Parks:</b>					
131 Brinkworthy Road	Private owner offered CRD a statutory right of way to develop a pedestrian pathway to connect Lower Ganges Road to the Brinkworthy community, alongside the western portion of the property along Brinkworthy Road.	In-Progress	50	Land survey identifying SRW and trail location has been completed and right of way registered on title. Project is funded in the 2023 capital plan.	Q3 2023
Trail Development - Trincomali	Development of a connector trail from Dunbabin Park to Trincomali Heights	On-Hold	25	Survey and environmental assessment now complete. Initial brush clearing partially complete. Project will resume during dryer months.	Q3 2023
Rotary Dock Replacement	Community Work Fund approved to cover the costs of a replacement dock	Complete	100	Dock replacement was completed February 2023	Q1 2023
Park Development (Pecks Cove)	\$150,000 of community works funds were allocated the boardwalk studies and design. Any remaining project funds can be used towards the revitalization of this park	On-Hold	0	On hold until completion of boardwalk designs.	
Fernwood Dock	Continue with maintenance and repairs as identified in the Condition Assessment report.	In-Progress	10	Complete underwater assessment and condition assessment to plan for capital works in 2024. Preventative maintenance scheduled for Q2 2023	Q4 2024
Centennial Park Plaza	Replace plaza and install lighting in Centennial Park	In-Progress	20	Grant funding has been secured, detailed designs complete. Project will be tendered in Spring 2023 with construction to begin in November 2023.	Q1 2024





Project	Description	Status	%	Comments	Timing
Centennial Boardwalk Repair	Repair or replace boardwalk and railing as identified in the 2021 condition assessment report.	In-Progress	5	Preventative maintenance scheduled for 2023. Requesting quotes for railing and piling repairs.	Q3 2023
Community Recreation:					
Program Development	Develop additional programming for seniors, adults, and youth	In-Progress	50	Program offerings continue to expand as classrooms at SIMS are renovated and new program supplies are purchased.	Q4 2023
Pool:					
Pool Electrical	Assessment of the condition of the electrical system	In-Progress	50	Assessment has been completed and so has a more detailed report with recommendations. Electrical room has been isolated and electrical equipment design is now complete. Tender has been awarded with replacement scheduled December 2023	Q1 2024
Pool Structural	Structural assessment for pool exterior wall	In-Progress	25	Assessment has been completed. Recommendations and costing to be brought back to LCC for consideration.	Q3 2023
Recreation Centre GHG Study	GHG reduction study for the Rainbow Road Recreation Centre	Complete	100	Consultant hired to identify a sequence of GHG reduction measures to achieve 50% GHG within the next 20 years. Final report and recommendation are now complete.	Q1 2023
Pool Mechanical	Replace heat pumps based on recommendations from GHG reduction and mechanical lifecycle report	In-Progress	10	Heat pump replacement scheduled for December 2023.	Q4 2023
EV Charger	Second EV charger to be installed at the Rainbow Road Pool	In-Progress	75	Electrical conduit to parking lot completed with portable installation. Grant received for new EV charger. EV installation soon to be completed	Q3 2023
Other					
Library	Upgrade cooling system	In-Progress	100	Project complete. Awaiting warmer weather to assess performance of system.	Q1 2023
Local Community Commission	Establishment of Local Community Commission (LCC) for Salt Spring Island	In-Progress	75	Voter approval obtained Oct/22. Dec/22 Bylaw 4507 adopted establishing an LCC. Election date set for May 27, 2023.	Q2 2023



Salt Spring Island Community Transit  
Monthly Revenue Report

	Jan-2022	Feb-2022	Mar-2022	Apr-2022	May-2022	Jun-2022	Jul-2022	Aug-2022	Sep-2022	Oct-2022	Nov-2022	Dec-2022	Total YTD
<b>Pharmasave 120</b>													
Monthly Passes - Adult	50.00	200.00	100.00	100.00	250.00	3150.00	-	-	-	-	-	-	850
Senior Student - Concession	240.00	200.00	160.00	240.00	240.00	6240.00	-	-	-	-	-	-	1,320
Day Passes	4.50	13.50	9.00	18.00	27.00	522.50	-	-	-	-	-	-	95
Ticket Sheets	800.00	560.00	260.00	660.00	260.00	41820.00	-	-	-	-	-	-	3,360
Total Sold Less Commission	1,072.61	954.03	518.42	997.64	761.46	1,207.85	-	-	-	-	-	-	5,512.01
<b>Blank - New Vendor</b>													
Monthly Passes - Adult	-	-	-	-	-	-	-	-	-	-	-	-	-
Senior Student - Concession	-	-	-	-	-	-	-	-	-	-	-	-	-
Day Passes	-	-	-	-	-	-	-	-	-	-	-	-	-
Ticket Sheets	-	-	-	-	-	-	-	-	-	-	-	-	-
Total Sold Less Commission	-	-	-	-	-	-	-	-	-	-	-	-	-
<b>Thrifty Foods</b>											Rec'd	Rec'd	
Monthly Passes - Adult	700.00	400.00	850.00	850.00	800.00	17850.00	-	-	-	-	-	-	4,450
Senior Student - Concession	400.00	520.00	560.00	520.00	480.00	15600.00	-	-	-	-	-	-	3,080
Day Pass	22.50	90.00	72.00	108.00	184.50	37166.50	-	-	-	-	-	-	644
Ticket Sheets	540.00	3,100.00	1,340.00	3,700.00	2,760.00	1422,840.00	-	-	-	-	-	-	14,280
Total Sold Less Commission	1,629.25	4,027.80	2,765.56	5,074.44	4,140.01	4,367.37	-	-	-	-	-	-	22,004.43
<b>Country Grocer</b>											Accrual	Accrual	
Monthly Passes - Adult	100.00	200.00	50.00	200.00	200.00	4200.00	-	-	-	-	-	-	950
Senior Student - Concession	80.00	160.00	160.00	120.00	80.00	280.00	-	-	-	-	-	-	680
Day Passes	-	22.50	31.50	-	4.50	418.00	-	-	-	-	-	-	77
Ticket Sheets	480.00	640.00	-	480.00	320.00	33660.00	-	-	-	-	-	-	2,580
Total Sold Less Commission	646.80	1,002.05	236.67	784.00	592.41	938.84	-	-	-	-	-	-	4,200.77
<b>SSI Parks &amp; Rec (Electoral area)</b>											Rec'd	Accrual	
Monthly Passes - Adult	-	-	-	50.00	-	-	-	-	-	-	-	-	50
Senior Student - Concession	80.00	40.00	40.00	-	40.00	-	-	-	-	-	-	-	200
Day Passes	-	-	-	-	-	14.50	-	-	-	-	-	-	5
Ticket Sheets	80.00	60.00	60.00	20.00	80.00	5100.00	-	-	-	-	-	-	400
Total Sold Less Commission	156.80	98.00	98.00	68.60	117.60	102.41	-	-	-	-	-	-	641.41
<b>SSI Mercantile (started Mar 2022)</b>												Rec'd	
Monthly Passes - Adult													-
Senior Student - Concession													-
Day Passes													-
Ticket Sheets	-	-	-	-	-	-	-	-	-	-	-	-	-
Total Sold Less Commission	-	-	-	-	-	-	-	-	-	-	-	-	-
<b>Total Vendor Sales</b>													
Monthly Passes - Adult	\$ 850.00	# \$ 800.00	# \$ 1,000.00	\$ 1,200	\$ 1,250	\$ 241,200	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 6,300
- Concession	\$ 800.00	\$ 920.00	\$ 920.00	880	840	23920	-	-	-	-	-	-	5,280
Day Passes	\$ 27.00	\$ 126.00	\$ 112.50	126	216	47212	-	-	-	-	-	-	819
Ticket Sheets *	\$ 1,900.00	\$ 4,360.00	\$ 1,660.00	4,860	3,420	2214,420	-	-	-	-	-	-	20,620
<b>Prepaid Fare Tickets</b>													
Collected from fareboxes * <i>(Island Savings Email from Sammie)</i>	2,722.50	- 2,693.25	- 2,902.50	2,081.25	3,906.00	3,863.25	-	-	-	-	-	-	18,169
<b>Farebox Cash Proceeds</b>	14,580.94	2,785.35	7,561.62	7,556.77	11,339.70	7,178.69							51,003
<i>(Island Savings Bank Statement)</i>													
<b>BC Bus Pass Program Grant</b>	2,291.35	2,319.26	2,291.56	2,253.27	2,279.34								11,435
<i>(Posted as Municipal Obligation ZVEN)</i>													
<b>Monthly Revenue</b>	\$ 21,271.79	# \$ 9,643.86	# \$ 14,788.18	# \$ 14,097.29	# \$ 19,831.04	67% \$ 13,373.44	0% \$ -	#### \$ -	#### \$ -	#### \$ -	#### \$ -	#### \$ -	\$ 93,005.60
<b>Cumulative Totals</b>													
YTD Revenue	\$ 21,271.79	\$ 30,915.65	\$ 45,703.83	\$ 59,801.12	\$ 79,632.16	\$ 93,005.60	\$ 93,005.60	\$ 93,005.60	\$ 93,005.60	\$ 93,005.60	\$ 93,005.60	\$ 93,005.60	95,456.85
Unearned Revenue (Ticket Sheets - Farebox)	(\$ 822.50)	\$ 844.25	(\$ 398.25)	\$ 2,380.50	\$ 1,894.50	\$ 2,451.25	\$ 2,451.25	\$ 2,451.25	\$ 2,451.25	\$ 2,451.25	\$ 2,451.25	\$ 2,451.25	
Less 2% Commission	(\$ 71.54)	(\$ 195.66)	(\$ 269.51)	(\$ 410.83)	(\$ 525.35)	(\$ 660.38)	(\$ 660.38)	(\$ 660.38)	(\$ 660.38)	(\$ 660.38)	(\$ 660.38)	(\$ 660.38)	(660.38)
Other Adjustments (SSI Commission)	(\$ 3.20)	(\$ 2.00)	(\$ 2.00)	(\$ 1.40)	(\$ 2.40)	(\$ 2.09)	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	-
<b>Reconciliation to GL</b>	\$ 20,374.55	\$ 31,562.24	\$ 45,034.07	61,769.39	80,998.91	94,794.38	94,796.47	94,796.47	94,796.47	94,796.47	94,796.47	94,796.47	94,796.47

<b>Reconciliation to GL - Total YTD</b>			94,796.47
ZVEN YTD Total		(96,388.87)	
Outstanding:	2022 Vendors payment received in 2023	4,651.08	
	Dec 2022 Municipal Obligation	2,344.44	
	O/S Pharmasave		
	O/S Thrifty's	(4,367.37)	
	O/S Country Grocer	(938.84)	
	O/S SSI		
	O/S SSI Mercantile		
	O/S Phone Transfer of Farebox Cash Proceeds	-	
Adj	SSI Difference: Remittance to BDC (see table)	(96.91)	
	ZVEN Adjusted Total		(94,796.47)
<b>Variance</b>			0.00

\* Unearned Revenue is the difference between Prepaid Fare Tickets sold and Prepaid Fare Tickets used by riders (collected from fareboxes).

Revenue Comparisons	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Total	Annual Incr
2023 Monthly Revenue	\$ 21,271.79	\$ 9,643.86	\$ 14,788.18	\$ 14,097.29	\$ 19,831.04	\$ 13,373.44	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 93,005.60	-
2022 Monthly Revenue	12,008	10,074	14,618	13,740	17,710	9,972	13,528	23,696	24,658	16,381	14,315	10,999	\$ 181,711.58	
2021 Monthly Revenue	7,250	9,483	10,899	11,195	12,100	10,875	13,820	19,155	15,485	10,664	11,680	9,481	\$ 142,087.36	28%



Salt Spring Island Community Transit  
Monthly Revenue Report

	Jan-2022	Feb-2022	Mar-2022	Apr-2022	May-2022	Jun-2022	Jul-2022	Aug-2022	Sep-2022	Oct-2022	Nov-2022	Dec-2022	Total YTD	
2020 Monthly Revenue	17,335	13,194	14,334	1,880	1,868	8,369	12,105	16,781	12,650	10,785	10,848	5,180	\$ -	13%
2019 Monthly Revenue	14,865	15,694	17,866	15,604	22,137	20,081	21,654	25,444	18,038	19,976	20,533	14,585	\$ 125,328.02	
2018 Monthly Revenue	12,723	13,648	18,592	14,537	17,296	22,920	20,574	26,116	17,970	16,380	18,075	13,174	\$ -	-45%
2017 Monthly Revenue	12,359	14,409	16,790	15,196	16,907	21,765	20,363	21,981	22,047	16,342	12,905	14,255	\$ 226,476.50	
2016 Monthly Revenue	12,324	13,571	12,677	15,760	19,891	16,364	18,522	23,998	20,363	13,412	13,422	16,663	\$ -	7%
2015 Monthly Revenue	11,658	14,067	-	16,096	15,920	15,081	-	19,681	-	20,181	-	15,471	\$ 212,004.80	
2014 Monthly Revenue	12,995	11,734	-	10,005	16,435	16,433	-	16,515	-	21,918	-	19,953	\$ -	3%
2013 Monthly Revenue	9,687	12,821	10,005	12,384	15,284	17,046	16,926	21,918	15,366	15,962	12,136	13,560	\$ 205,318.23	
2012 Monthly Revenue	9,578	8,662	14,540	10,240	14,945	21,766	7,030	21,926	15,374	12,785	12,868	11,650	\$ -	4%
2011 Monthly Revenue	4,899	5,540	6,822	6,043	7,221	10,116	10,954	15,731	11,024	8,650	7,912	9,737	\$ 196,967.57	
2008 Monthly Revenue	1,942	3,734	4,074	4,635	5,871	6,493	9,318	10,228	8,612	7,192	5,895	8,810	\$ -	-3%
													\$ 202,919.42	8%
													\$ 188,586.07	9%
													\$ 173,093.96	7%
													\$ 161,364.87	54%
													\$ 104,648.25	14%
													\$ -	
													\$ 76,803.48	

Other Adjustments

Commission Refund														
GL adjustments														
SSI Commission (Over)/Under pmt	3.20	2.00	2.00	1.40	2.40	2.09	-	-	-	-	-	-		
Total	3.20	2.00	2.00	1.40	2.40	2.09	-	-	-	-	-	-		

**REPORT TO SALT SPRING ISLAND LOCAL COMMUNITY COMMISSION  
MEETING OF TUESDAY, JULY 18, 2023**

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**SUBJECT**     **2024 Grants-in-Aid Application and Distribution of Funds**

**ISSUE**

To review the current administration and distribution of Grants-in-Aid funding to streamline the granting process and increase an equitable distribution of funds.

**BACKGROUND**

Grants-in-Aid (GIA) are one-time grants funded through tax requisition to fund special projects and activities, awarded to community non-profit organizations that deliver projects benefiting electoral areas (Juan de Fuca, Salt Spring Island and Southern Gulf Islands) that are not already funded by requisition.

GIA's are one-time grants awarded to projects or activities beyond the scope of Capital Regional District (CRD) services and represent an appropriate use of tax dollars to benefit an electoral area.

*Eligibility Criteria:*

- Grants-in-Aid are intended for non-profit agencies or societies for projects of benefit to the electoral area.
- Grants-in-Aid are intended for special one-time projects, not ongoing operational funding, although applications for recurring projects can be made.
- Organizations already being funded directly on an ongoing basis by taxation, or industrial, commercial or business undertaking or private individuals **are not** eligible for assistance under this program.
- Grants-in-Aid **are not** intended for the personal benefit of any individual, proprietor, member or shareholder.

Currently, GIA's are received throughout the year and are awarded at the discretion of the Salt Spring Island Electoral Area Director, provided they meet the eligibility criteria assessed by staff. With the establishment of Bylaw No. 4507, the Salt Spring Island Local Community Commission (LCC) now approves the issuance of GIA's that meet the eligibility criteria for Salt Spring Island Initiatives subject to the Director's approval to advance the GIA to the LCC.

Currently, applications are reviewed on a first-come, first-served basis, and GIA funding can be depleted before the end of the year. At times the GIA budget had year-end surpluses, which demonstrated over-budgeting. Providing a deadline for GIA applications will allow the LCC to review all applications to assess a more equitable distribution of funds. It will assist the LCC in determining community needs and ensure an annual amount can be incorporated in the annual budget. Staff time to review and process approved GIA applications at the same time would result in more efficiencies and reduced administration related to:



- Review and screening of ad hoc applications against eligibility criteria
- Providing staff recommendations for funding requests
- Confirming GIA funding balances
- Processing approved applications and issuing funds;

The CRD Board approves annual budgets including GIA in March of each year. Providing a deadline for GIA applications in the prior year will assist the LCC in the following ways:

- Ensuring sufficient budgeting for GIA's are incorporated in the annual budget.
- Reduce the risk of running out of GIA funding; or over budgeting
- Assessing community benefits, program, project quality and merit in the distribution of GIA funding
- Organizational effectiveness and capacity
- Community engagement and impact
- Having applicants demonstrate sound and financial, and administrative management
- Assisting applicants to prepare and anticipate funding needs for their annual budget

An annual GIA application deadline will allow staff to prepare a staff report with all applications received for LCC consideration. A publicized deadline would also indicate to potential applicants to submit their proposals for the following year. Recognizing that unique and beneficial special events may arise from time to time outside a set timeline, the LCC can budget a certain amount for grants received after the deadline that can be dealt with on an ad hoc basis.

## **ALTERNATIVES**

### ***Alternative 1***

The Salt Spring Island Local Community Commission (LCC) recommends that a Salt Spring Island Grant-in-Aid Eligibility Criteria include an annual deadline of April 30 for Salt Spring Island Grant-in-Aid applications.

### ***Alternative 2***

This report be referred back to staff for additional information.

## **IMPLICATIONS**

Having a deadline for GIA applications will support the annual budgeting process and ensure adequate funding in the five-year financial plan. The process will support LCC deliberations on evaluating GIA requests and how best to award funding to organizations that request financial assistance based on the services the organization provides to the community, the aggregate amount of financial assistance required, total benefits that will result from the grant, along with the details from the organization's proposed budget for the current year, detailing expenditures, revenues and including a list of other grants, or donations, and the financial statements from the immediate preceding fiscal year.

## **CONCLUSION**

Currently, community organizations submit requests for funding for one-time projects and special events throughout the year and applications are reviewed by staff to determine eligibility criteria before approval by the Electoral Area Directors. With the establishment of the LCC there is an opportunity to streamline the application process for Salt Spring Island GIA applications to reduce

administrative responsibility and increase an equitable distribution of funds.

**RECOMMENDATION**

That the Salt Spring Island Local Community Commission (LCC) recommends that a Salt Spring Island Grant-in-Aid Eligibility Criteria include an annual deadline of April 30 for Salt Spring Island Grant-in-Aid applications.

Submitted by:	Karla Campbell, BPA, MBA, Senior Manager, Salt Spring Electoral Area
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**REPORT TO LOCAL COMMUNITY COMMISSION  
MEETING OF DAY, JULY 18 2023**

**SUBJECT     GRANTS-IN-AID – COPPER KETTLE**

**ISSUE**

To approve the grant-in-aid application for the Electoral Areas.

**BACKGROUND**

The Supplementary Letters Patent for grants-in-aid require the Capital Regional District (CRD) approval be obtained before any payments for grants-in-aid are made on behalf of one or more member electoral areas to any organization deemed to be contributing to the general interests and advantage of the area.

This service covers the Electoral Areas. Before exercising the powers described above, the Supplementary Letters Patent requires that the Salt Spring Island (SSI) Local Community Commission (LCC) obtain the written approval of the Director of each electoral Area to the proposed grant-in-aid for such Electoral Area. The Director has approved the GIA to be considered by the LCC.

**FINANCIAL IMPLICATIONS**

The proposed application is a recurring grant request since 2015. According to the Grant-in-Aid guidelines an indication of whether or not the project is recurring, is *“most grant in aid requests are for one-time projects. On rare occasions, projects have been of a recurring nature (e.g. JDF Parent Advisory Councils requesting funding for a crossing guard to assist children in crossing a high traffic road). If a project is recurring, it is important to ensure that it isn’t something that would be more appropriately addressed by establishing a service”*. The applicant does not have financial statements nor a project budget to attach to their application.

2023 GIA Overall Summary	Core GIA				Safe Restart GIA Carry over 2023			Total Remaining Core + SR
	Budget	Spent	Committed*	Remaining	Budget	Spent	Remaining	
SSI (\$)	43,484	26,000	10,000	7,484	10,000	5,000	5,000	12,484 <sup>1</sup>

2023 PROJECT SUMMARY		
Applicant	Project Title	Amount Awarded (\$)
Capital Bike	Everyone Rides SSI	3,475
Salt Spring Chamber of Commerce	Ganges Garbage and Recycling Pilot Program	5,000
Salt Spring Community Alliance	Goodwill Ambassador Program	5,000
Salt Spring Community Energy	Electricity Salt Spring Month of Events	4,600
Salt Spring Island Weavers and Spinners Guild	Support for Basic Furnishings	1,000
Salt Spring Literacy	Learning Space Equipment	1,000
Transition Salt Spring Island	Support for Furnishings and Equipment	925

Salt Spring Island Chamber of Commerce	Summer Outdoor Concert Series 2023	4,000
Salt Spring Island Printmakers Society	Furnishings for SSI Printmakers Society Studio Space	1,000
	<b>Core GIA Total</b>	<b>26,000</b>
Diverse and Inclusive Salt Spring Island Society	Salt Spring Pride Festival	5,000
	<b>Safe Restart GIA Total</b>	<b>5,000</b>

1. *2022 Committed Funds: Salt Spring Community Health Society: \$10,000 for "Electric Mobile Health Van." As per Director Holman, this project was conditionally approved in 2022 upon applicant demonstrating securing of other external funding. The funds have not yet been released.*

## **ALTERNATIVES**

That the Salt Spring Island Local Community Commission:

1. Approve a grant-in-aid to Copper Kettle in the amount of \$5,000.00
2. Award a different amount.
3. Refer the grant-in-aid back to staff for more information

## **RECOMMENDATION**

That the Salt Spring Island Local Community Commission approve a grant-in-aid to Copper Kettle in the amount of \$5,000.00

Submitted by:	Karla Campbell, MBA, BPA, Senior Manager, Salt Spring Island Administration
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Appendix A: Grant-In-Aid Application





Making a difference...together

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**Capital Regional District**  
**Grant-In-Aid Application Form**  
**FOR ELECTORAL AREAS ONLY:**

**Juan de Fuca**

**Salt Spring Island**

**Southern Gulf Islands**

[Revised November 16, 2012]



## CRD GRANT-IN-AID APPLICATION FORM

Grants-in-Aid are for the CRD Electoral Areas only: Juan de Fuca, Salt Spring Island, and Southern Gulf Islands

### Application Submitted By

Name and Address of Applicant: COPPER KEATLE COMMUNITY PARTNERSHIP  
C/O 151 LAWNHILL DR  
SS1 V8K 1M9

Contact(s): CHERIE BEAUVREAU 250 537 5863  
(name) (phone) (fax)

Email address

Contact(s): JEWEL ENLSTROM 250 537 1027  
(name) (phone) (fax)

Email address

### Application Summary

Project or purpose for which you require assistance:

Amount of grant requested \$ 5000.00

To the best of my knowledge, all of the information that is provided in this application is true and correct. Furthermore, I hereby certify that this application for assistance is:

- NOT being made on behalf of an industrial, commercial, or business undertaking
- NOT available for the personal benefit of any individual, proprietor, member or shareholder

Cherie Beauveau  
(signature of authorized signatory)

-ADMINISTRATOR  
(title)

CHERIE BEAUVREAU  
(print name)

## Applicant Profile

1. Please describe the services / benefits that your organization provides to the community. Are these services / benefits available to the community from another organization or agency?

FOOD GIFTS TO THOSE IN NEED of  
NOURISHMENT.

2. Describe the geographic area that receives services or benefits from your organization.

SALT SPRING ISLAND

3. Is your organization voluntary and non-profit? ☐ NO ☒ YES

Please detail any remuneration paid, or funds otherwise made available to members, officers, etc. of your organization.

Ø

(OCCASIONAL GAS REIMBURSEMENT)

Please indicate the number of members / volunteers in your organization and how long your organization has been in operation.

SINCE 2002 - 21 YEARS

9+



### Project / Proposal Profile

1. Assistance is being requested for: FOOD GIFTS FROM COUNTRY GROCER  
capital project and / or equipment  
special event  
other purpose ( \_\_\_\_\_ )
2. Please describe the proposal for which you are requesting assistance. Attach additional information if required.

100 X \$50 GIFT CARDS = \$5000

3. Please describe how this proposal will benefit the community.

100 PEOPLE LESS HUNGRY

## Funding and Financial Information

1. Attach supporting financial information, i.e., budget / financial report. Ensure the following information is clearly itemized;
  - o project budget, \$5000.
  - o grants / funding from other sources, SAVE A TAME COUNTRY GARDEN
  - o funding contributed by applicant through fund raising activities or other sources of revenue and,
  - o financial statement that itemizes total expenses for the fiscal year, including any monies and/or benefits paid to members or officers. N/A

2. Have you applied for a grant / funding from another source(s)? ☒ NO ☐ YES

If yes, complete the following chart. If no, please explain \_\_\_\_\_

Name of Grant or Funding Agency	\$ Amount Applied For	Status of Grant Application		
		Approved (Y)	Denied (Y)	Pending (Y)
		<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
		<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
		<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
		<input type="radio"/>	<input type="radio"/>	<input type="radio"/>

3. Have you received assistance (grant in aid / waiving of fees, etc.), from the CRD in previous years?

☐ NO ☒ YES..... If yes, please complete the following chart.

Year	\$ Amount	Purpose for which assistance was used
2022	5000	FOOD GIFTS FOR THE UNDERNOURISHED *
↓	✓	
2015	✓	

\* FIRST 3 MONTHS of 2023 95 + 2 97 CARDS  
@ \$50. \$4850  
FROM DONATIONS & BBA'S

4. Does your organization:

Offer direct financial assistance to individuals or families?

☐ Yes ☒ No

Provide services that fall within the mandate of either  
a senior government or a local service agency?

☐ Yes ☒ No

Provide an opportunity for individuals to make direct  
Contributions to the project (e.g., fundraising for the project?)

☒ Yes ☐ No

Or, is your organization:

part of a Provincial or National fund raising campaign?

☐ Yes ☒ No

The information provided in Section 4 is for data collection purposes.

**Followup:**

Please refer to Page 6 of the ***Grant-In-Aid - Application Completion Guide*** regarding the following:

**1. Acknowledgement**


**2. Reporting**

***Please remember to attach:***

1. Project budget
2. Financial statement for your organization



# Terms of Reference

The logo for the Capital Regional District (CRD) is located in the top right corner of the header. It consists of the letters 'CRD' in a stylized, bold, sans-serif font, positioned above a series of three curved, overlapping lines that suggest a landscape or water.

## **Ganges Harbour Walk Project Steering Committee**

### **PREAMBLE**

The Capital Regional District (CRD) Ganges Harbour Walk Project Steering Committee is an advisory committee established by the CRD Board and will oversee and make recommendations to the Salt Spring Island Local Community Commission (LCC) regarding matters related to the construction of the Ganges Harbour walk.

The Committee's official name is to be:

Ganges Harbour Walk Project Steering Committee (GHWPSA)

### **1.0 PURPOSE**

The mandate of the Committee includes overseeing and making recommendations to the LCC regarding the following:

- Responsible for securing permanent funding for the development and construction of the project by way of fund raising, grant writing, and if necessary, championing referendum financing process.
- Ensures value for the project by achieving a return on investment by:
  - Completing project on time and on budget;
  - Future annual maintenance and operating costs, including life cycling costing are minimized;
  - Enhancing the safety, aesthetics, and accessibility of the harbour walk; and
  - Meeting community needs and objectives for years to come
- Provide the Project Manager with requirements concerning decisions or policies that may impact scope or project outcomes.
- Provide advice and feedback on scope, schedule, cost and quality concerns, or guidance on project priorities that arise during the planning, design and implementation of the project.
- Provide issue resolution on other matters referred to the Committee by the Project Manager and/or staff liaison.
- Facilitate project approvals at key milestones: procurement awards, site and engineering assessment, design development.
- Provide the Project Construction Manager with advice and direction where appropriate concerning site priorities for renovation/upgrade of the existing boardwalk structure or realignment and joining the boardwalk to Pecks Cove Park and southern entrance to the harbour walk.

## **2.0 ESTABLISHMENT AND AUTHORITY**

- The Committee in an advisory capacity will make recommendations to the LCC on matters impacting a change in scope, financing, or procurement award as required in the CRD Procurement Policy.
- The LCC Chair will appoint the Committee Chair and Committee members for the duration of the project as per section 3.0.
- Upon completion of the project the Committee will be disbanded.

## **3.0 COMPOSITION**

- Committee members will be appointed as follows:
  - Salt Spring Island Electoral Area Director
  - One LCC commissioner, nominated by the LCC
  - One Salt Spring Island Trustee nominated by the Local Trust Committee
  - One Board member nominated by the Salt Spring Island Chamber of Commerce.
  - One member at large nominated by the Salt Spring Island Electoral Area Director.

## **4.0 PROCEDURES**

- The Committee shall meet as needed or at the call of the Committee Chair, in consultation with the staff liaison.
- The agenda will be finalized in consultation between staff liaison and the Committee Chair, and any Committee member may request that a matter be placed on the agenda.
- For meetings to proceed a quorum of the committee will be required to be in attendance to conduct committee business. A quorum is a minimum of three members.
- With the approval of the Committee Chair, committee matters of an urgent or time sensitive nature may be forwarded to LCC for consideration.

## **5.0 RESOURCES AND SUPPORT**

- The Salt Spring Island Senior Manager will act as liaison to the Committee.
- The Salt Spring Island Electoral Area Administrative division will provide administrative support as required; and prepare and distribute minutes and agendas.

- A Project Construction Manager will be assigned by the CRD to lead the team responsible for achieving the project scope, objectives, change and issue management, risk assessment, procurement, and communication on project status, including problems and issues that require the Committee direction.



Making a difference...together

Executive Office

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Victoria, BC V8W 2S6

P: 250.360.3125

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[www.crd.bc.ca](http://www.crd.bc.ca)

June 16, 2023

File 8310-01

SSI Electoral Area Administrator  
JDF Electoral Area Administrator  
SGI Electoral Area Administrator  
*Via email*

Dear Karla, Iain and Stephen:

**RE: Capital Regional District Board Engagement on Transportation Governance**

As you are aware, transportation is a priority for the 2023 - 2026 Capital Regional District (CRD) Board term. A key initiative identified through the Board's strategic planning process is to present options for changes in governance for transportation in the capital region, including the three electoral areas.

Since adopting the Regional Transportation Plan (RTP) in 2014, the CRD and partners have worked together to develop local transportation plans and implementation strategies to align our long-term goals. We have ambitious climate action and transportation targets. While much is being done, in the absence of a unified and coordinated approach, funding to continue developing the regional multimodal network is unavailable.

At the June 14, 2023, CRD Board meeting, the Board directed staff to gather input from local governments, electoral areas, the province, and relevant partner agencies, including the Island Corridor Foundation, on transportation governance. The Board needs our partners' valuable insight into challenges, expectations, and aspirations for transportation governance in the capital region. An engagement approach has been developed to build consensus on the scope and scale of governance change needed to achieve regional mobility objectives as set out in the RTP.

A transportation governance engagement workbook will be provided to you in mid-July. We ask that one workbook be completed, endorsed by your Electoral Area Director, and returned to the CRD before September 15, 2023. Your participation in completing this book and providing feedback is crucial in advancing this CRD Board Priority this term.

CRD staff would be pleased to meet with you on the governance concepts and engagement strategy. If you are interested in having CRD staff present, please contact Alesha Hayes at [ahayes@crd.bc.ca](mailto:ahayes@crd.bc.ca) to arrange a time.

We look forward to your support on this matter.

Sincerely,

Ted Robbins  
Chief Administrative Officer

cc: Kevin Lorette, General Manager, Planning and Protective Services