

Salt Spring Island Active Transportation Network Plan

July 2023



Acknowledgements

Land Acknowledgement

The Capital Regional District respectfully acknowledges that we live and work within the ancestral and unceded traditional territory of the Hul'qumi'num and SENĆOŦEN speaking peoples.

Grant Acknowledgement

Funding for the Active Transportation Network Plan has been made possible through the BC Active Transportation Infrastructure Grants Program from the Ministry of Transportation and Infrastructure.

Project Acknowledgements

WATT Consulting Group and Ahne Studio would like to thank the residents and visitors of Salt Spring Island, CRD staff, the Salt Spring Island Transportation Commission, and other stakeholders who provided invaluable feedback throughout the plan process.

The Active Transportation Network Plan was developed in a collaborative fashion, involving members of WATT Consulting Group, Ahne Studio, and the CRD. The following team members contributed to this project.

WATT Consulting Group

Tim Shah, RPP, MCIP
Project Lead & Sr. Transportation Planner

Jimin Park, MCRP
Project Co-lead & Transportation Planner

Andy Kading, P.E., P.Eng
Sr. Transportation Engineer

Simon Lesiuk
Engineer-in-Training

Adrian Myszka
Transportation Planner

Capital Regional District

John Hicks, RPP, MCIP
Senior Transportation Planner

Karla Campbell, MBA, BPA
Senior Manager, Salt Spring Island Administration

Dean Olafson, P.Eng, MBA
Engineering Manager, Salt Spring Island Administration

Ahne Studio

Jennifer Fix, RPP, MCIP
Project Engagement Lead & Principal

Table of Contents

Executive Summary	6
1.0 Introduction	10
1.1 Purpose	10
1.2 Plan Process	11
1.3 What is Active Transportation?	11
2.0 Community Context	12
2.1 Location	12
2.2 Demographic Highlights	12
2.3 How Salt Spring Islanders Get Around Today	14
2.4 Challenges Unique to Salt Spring	16
2.5 Policy Context	18
3.0 Public & Stakeholder Engagement	20
4.0 Plan Framework	24
4.1 Vision	24
4.2 Guiding Principles	25
4.3 Strategy Areas	26
4.4 Key Infrastructure Definitions	27
5.0 Strategy Area 1: Reallocate Street Space for Walking, Rolling and Cycling	28
6.0 Strategy Area 2: Make Ganges Village Accessible for all Residents and Visitors	52
7.0 Strategy Area 3: Encourage Sustainable Modes	64

8.0

Implementation Strategy

72

8.1

Big Moves

72

8.1.1

Big Move 1

73

8.1.2

Big Move 2

73

8.1.3

Big Move 3

74

8.1.4

Big Move 4

74

8.1.5

Big Move 5

75

8.2

Capital Planning

76

8.2.1

The Big Moves

76

8.3

Operational & Maintenance Considerations

77

8.4

Action Plan

78

8.5

Funding Opportunities

81

8.5.1

CRD Funding

81

8.5.2

BC Active Transportation Infrastructure Grant Program

82

8.5.3

Green Municipal Funds

82

8.6

Next Steps

83

This Page Intentionally Left Blank

Executive Summary

Overview

In 2022, the Capital Regional District (CRD) obtained funding from the BC Active Transportation Grants Program to prepare an Active Transportation Network Plan (ATNP) for Salt Spring Island (SSI). The ATNP, which builds on Salt Spring's 2013 Pedestrian and Cycling Master Plan, is intended to be a practical, cost-effective, and technically rigorous planning document that will guide active transportation planning in Ganges Village over the next 10 years. Broadly speaking, the purpose of the ATNP is three-fold:

1. Understand and document the existing challenges and barriers to active transportation within Ganges Village;
2. Provide a list of high-priority active transportation infrastructure projects for Ganges Villages that can improve quality of life, safety, and mobility for residents and visitors; and
3. Identify the policy and programmatic actions that are needed to promote active transportation and support emerging mobility options that respond to the needs of island residents.

WATT Consulting Group, in partnership with Ahne Studio and the Capital Regional District, undertook a 10-month planning process to produce the ATNP. The plan included a public and stakeholder engagement process, a detailed technical analysis, and engineering considerations to ensure that the proposed recommendations are actionable and achievable. The ATNP included two key deliverables that set the foundation for the recommendations:

1. **Baseline Conditions Report (November 2022):** the report provided a detailed analysis of Salt Spring Island's current transportation network. This included an overview of existing transportation mode share and travel patterns; a summary of the road network; a summary of the main barriers to walking, rolling, and cycling; and an outline of the key opportunities to be furthered explored in the ATNP through public engagement and additional technical analysis.
2. **Phase 2 Engagement Summary (November 2022):** the report summarized the initial engagement activities, where the community was asked to share their feedback on the barriers, issues, and opportunities to improve active transportation on the island. The engagement findings largely validated the technical analysis completed in the Baseline Conditions Report and allowed the project team to understand the overall priorities.

A Focus on Ganges Village

While the ATNP was being prepared over the course of 2022, the Ministry of Transportation and Infrastructure was simultaneously undertaking the Salt Spring Island Cycling Safety Review. The purpose of that study was to explore the improvements needed for cycling safety on the major roadways that connect the ferry terminals on Salt Spring including Fulford-Ganges Road, Lower Ganges Road, and Vesuvius Bay Road. The study identifies several safety issues on the three major corridors and includes mitigation measures that span addressing speeding concerns, the provision of new cycling facilities, and intersection designs.

Based on the scope of the Salt Spring Island Cycling Safety Review, and to avoid duplicating any efforts, the Active Transportation Network Plan is focused on Ganges Village and does not provide recommendations

for improvements across the island. Further, the focus on Ganges Village is consistent with the original scope of work requested by the CRD recognizing that the 2013 plan did not provide detailed direction on the improvements needed for the village.

Vision & Guiding Principles

A vision statement was created for the Active Transportation Network Plan to capture what Ganges Village aspires to be in 10 years from now. The vision statement was informed by the public engagement undertaken as part of the ATNP and reflects the broader transportation themes from the Ganges Village Local Area Plan process.

Ganges Village is a place to gather, where the community—including residents, visitors, and those employed in the local economy—can move around the village seamlessly, safely, and actively by any mode of active transportation. The abundance of communal gathering spaces reflects the vibrancy and livability of Ganges Village. The active transportation network has been transformed, where a larger share of the roads has been reallocated to people walking, rolling, and cycling. All destinations within the village are easily accessible for all users, especially those with mobility challenges. Ganges Village is connected to the larger island, accessible, and safe for all where active transportation is the preferred way to move around—lowering the community's GHG emissions, contributing to the local economy, and allowing Ganges to realize its full potential as a complete community and as a cultural tourism destination.

Five guiding principles were developed for the Active Transportation Network Plan. They reflect the values of active transportation planning on Salt Spring and serve as the framework for the plan's recommendations.

1. **Low-cost, Quick-build, & Actionable Solutions:** A quick-build approach is always explored first for all active transportation improvements to minimize costs and build new projects in a time efficient manner.
2. **A Pedestrian Friendly Village:** Ganges Village is a pedestrian friendly village that maximizes space for people. The Village is designed in a way that maximizes safety and comfort for those walking and rolling while retaining on-street parking where possible.
3. **Integration & Connectivity:** Ganges Village is part of the island's larger active transportation network that enables safe and convenient connections to key community destinations.
4. **A More Accessible Ganges:** An active transportation environment that enables residents and visitors of all abilities to walk and roll safely, especially those with mobility aids or strollers.
5. **Active Transportation Solutions are Climate Solutions:** Consistent with the CRD's Regional Climate Action Strategy, new active transportation infrastructure can mitigate climate impacts where on-road transportation represents the largest source of the region's greenhouse gas (GHG) emissions. All active transportation solutions will be positioned, framed, and articulated in the larger context of meeting the CRD's commitment to reduce GHG emissions by 40% by 2030.

Strategic Framework

Strategy Area 1
Reallocate Street Space
for Walking, Rolling, and
Cycling

- Action 1A:
Implement McPhillips Avenue pedestrian facilities
- Action 1B:
Implement Rainbow Road pedestrian and cycling facilities
- Action 1C:
Implement Fulford-Ganges Road pedestrian and cycling facilities
- Action 1D:
Implement Lower Ganges Road pedestrian and cycling facilities
- Action 1E:
Adopt infrastructure design guidelines
- Action 1F:
Connect to active transportation network outside of Ganges Village

Strategy Area 2
Make Ganges Village
Accessible for all Residents
and Visitors

- Action 2A:
Conduct a land ownership survey of the Ganges Village
- Action 2B:
Adopt a 30 km/h posted speed limit in the core of Ganges Village
- Action 2C:
Create village gateways with traffic calming design interventions
- Action 2D:
Undertake reviews of intersections within Ganges Village
- Action 2E:
Retain accessible parking within the Village
- Action 2F:
Establish no-parking zones with physical barriers where necessary

Strategy Area 3
Encourage Sustainable
Modes

- Action 3A:
Develop a bike parking retrofit program
- Action 3B:
Identify opportunities to increase off-street parking
- Action 3C:
Develop a parking wayfinding program
- Action 3D:
Work with BC Transit to integrate active transportation modes

Big Move

The Five Big Moves

A total of 26 actions are recommended in the ATNP. The implementation of each action will require time, resources, and financial commitments. The recommended sequencing of the actions is shown in Section 8.3 (Action Plan). While all the actions in the ATNP are critical for helping meet the plan’s vision, some are deemed to be of higher importance to have a more immediate and direct impact on Ganges’ active transportation network.

The five big moves were selected based on meeting at least one of the following criteria:

- 1. **Low-cost.** In alignment with the first guiding principle of the ATNP, a big move should be relatively low cost recognizing the limited financial resources for infrastructure on the island.
- 2. **Actionable.** In alignment with the first guiding principle of the ATNP, a big move should be actionable, meaning that is requires minimal staff time and strong support from key stakeholders such as MOTI to make implementation seamless.
- 3. **High Impact.** High impact refers to increasing the number of people walking, cycling, and rolling while improving safety and accessibility for the most vulnerable road users.

The “big moves” are identified in the strategic framework with their corresponding action number. The big moves, presented in detail in Section 8.1, have been listed based on their order of priority reflecting the criteria above. Ultimately, however, the CRD and its partners will need to determine what is most feasible to implement first based on funding opportunities, staffing, and overall resources.

Implementation Strategy

The five “big moves” are recommended to be implemented first. The overall cost for big moves 1, 4, and 5, is anticipated to be relatively low and will depend on several factors such as overall signage requirements, the complexity of the land ownership survey, and whether the CRD acquires land for a new off-street parking location. Based on the Class D cost estimates, the total cost of Big Move 2 (Action 1A) and Big Move 5 (Action 1C) is approximately \$432,000.

The total level of investment for all of the proposed active transportation corridor improvements is approximately \$1,647,000 excluding GST. The implementation strategy includes an overall action plan for each of the recommendations in the ATNP and the different funding opportunities available to pay for the facility improvements.

Next Steps

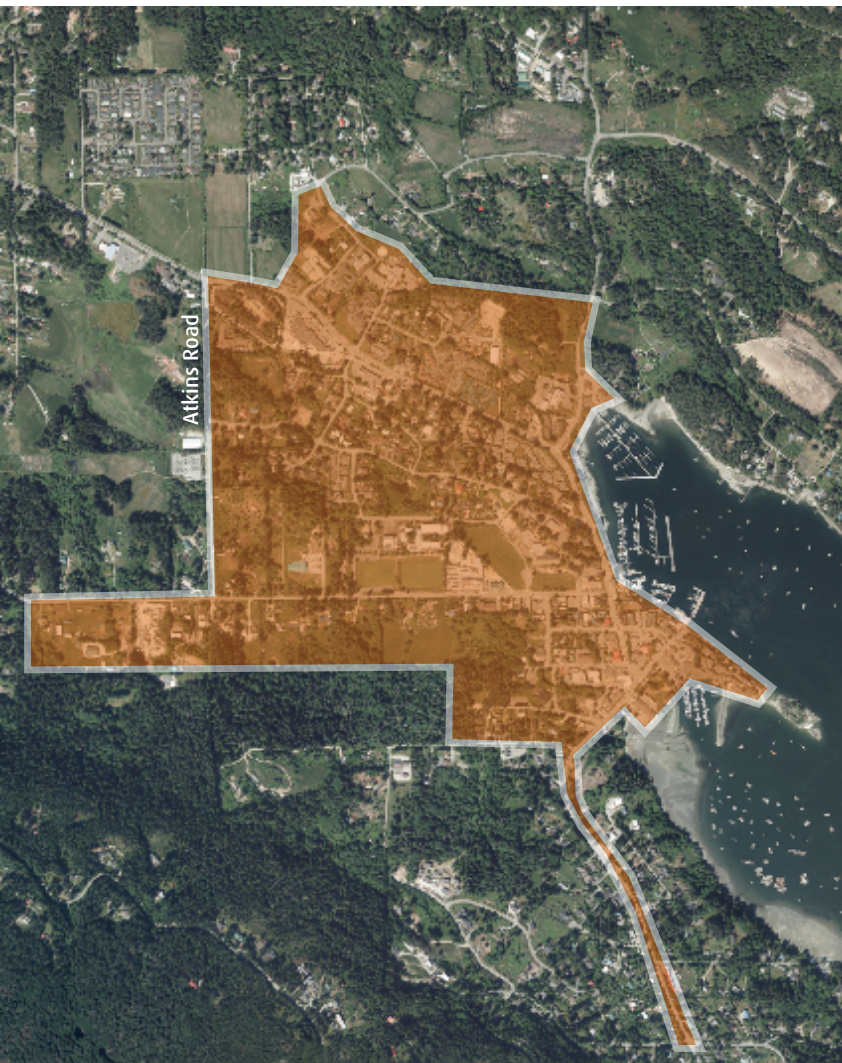
The ATNP is intended to move the community on a path toward better, safer, and more inclusive active transportation. As a smaller community with limited financial and staff resources, the CRD, and its associated partners, will need to move quickly on the low-cost, quick-build solutions identified in the ATNP. Further, the ATNP should be a living document and not sit on the proverbial bookshelf collecting dust. It is recommended that the plan be revisited in 5 years (2028) to explore what has—and has not—been implemented to date and whether any of the actions in this plan need to be reprioritized. A more walkable, bike friendly, and accessible Ganges Village will take time, and is anticipated to boost the local economy, lower the community’s GHG emissions, and enhance safety for all.

1.0 Introduction

1.1 Purpose

In 2022, the Capital Regional District (CRD) obtained funding from the BC Active Transportation Grants Program to prepare an Active Transportation Network Plan (ATNP) for Salt Spring Island (SSI). The ATNP, which builds on Salt Spring's 2013 Pedestrian and Cycling Master Plan, is intended to be a practical, cost-effective, and technically rigorous planning document that will guide active transportation planning on the island over the next 10 years. Broadly speaking, the purpose of the ATNP is three-fold:

1. Understand and document the existing challenges and barriers to active transportation within Ganges Village;
2. Provide a list of high-priority active transportation infrastructure projects for Ganges Villages that can improve quality of life, safety, and mobility for residents and visitors; and
3. Identify the policy and programmatic actions that are needed to promote active transportation and support emerging mobility options that respond to the needs of island residents.



Ganges Focus

The Active Transportation Network Plan is focused on Ganges Village and does not provide recommendations for improvements across the island. The focus on Ganges Village is consistent with the original scope of work requested by the CRD, recognizing that the 2013 plan did not provide detailed direction on the improvements needed for the village. Further, while the ATNP was being prepared over the course of 2022, the Ministry and of Transportation and Infrastructure was undertaking the Salt Spring Island Cycling Safety Review. The study's purpose was to explore the improvements needed for cycling safety on the major roadways that connect the ferry terminals on Salt Spring including Fulford-Ganges Road, Lower Ganges Road, and Vesuvius Bay Road. The study identifies several safety issues on the three major corridors and includes mitigation measures that span addressing speeding concerns, the provision of new cycling facilities, and intersection designs.

To avoid duplicating the analysis and recommendations found within the Salt Spring Island Cycling Safety Review, the Salt Spring Island ATNP does not include any recommendations on the three major corridors but does address the need to enhance connectivity from Ganges Village to those corridors.

1.2 Plan Process



Phase 1
Understanding



Phase 2
Listening



Phase 3
Developing



Phase 4
Finalizing

The Salt Spring Active Transportation Network Plan launched in July of 2022 and was led by WATT Consulting Group in collaboration with Ahne Studio, the Capital Regional District, key stakeholders, and community members. Through a highly collaborative planning process, the voices of community members and transportation experts were captured to help shape the future of active transportation with Ganges Village. The plan was created in four distinct phases, which are detailed below:

The first phase, **Understanding**, culminated with the Salt Spring Island ATNP Baseline Conditions Report that included a detailed analysis of the barriers and opportunities around active transportation within Ganges Village and across the island.

The second phase, **Listening**, included the initial engagement work, where the community was asked to share their feedback on the barriers, issues, and opportunities to improve active transportation on the island. The community feedback was summarized in the Salt Spring Island ATNP Phase 2 Engagement Summary.

The third phase, **Developing**, included a detailed assessment of the potential active transportation corridors in Ganges; producing the draft vision and guiding principles; and establishing the key recommendations and “big moves” for the ATNP. The Developing phase also included additional engagement with the community where a stakeholder workshop and online survey were undertaken to collect feedback on the draft ATNP vision, guiding principles, and preliminary recommendations.

The fourth and final phase, **Finalizing**, included finalization of the ATNP and a presentation to the Local Community Commission.

1.3 What is Active Transportation?

The most comprehensive definition of active transportation in the BC context is provided in the BC Active Transportation Design Guide (BC ATDG). It is defined as follows:

“Any form of human-powered transportation, including walking, cycling, or rolling using a skateboard, in-line skates, wheelchair, or other wheel-based forms of human-powered transportation. It also includes winter-based active modes, water-based active modes, and horseback riding, although these modes are typically more recreational in nature.” Active transportation can also include emerging mobility (or new mobility) travel modes made possible by advances in technology including electric bicycles and electric scooters.

According to the guide, active transportation should be based on inclusive mobility to ensure that the network is comfortable, convenient, safe, and attractive for everyone, regardless of age or ability. A full definition of active transportation, including its benefits and key principles of inclusive mobility, is provided in Chapter B of the BC ATDG.

2.0 Community Context

2.1 Location

Located on the ancestral and unceded traditional territories of the Hul'q'umi'num and SENĆOŦEN speaking peoples, Salt Spring Island is the largest and most populous of the Southern Gulf Islands within the Capital Regional District. Salt Spring is serviced by BC Ferries along three routes linking Fulford Harbour with Swartz Bay, Vesuvius to Crofton, and Long Harbour to Tsawwassen.

2.2 Demographic Highlights

As of 2021, Salt Spring was home to 11,635 year-round residents with the population frequently doubling in the summer with an influx of visitors. The island has experienced rapid population growth in the last five years as compared to the previous census period. Additionally, the percentage of those in the 60 years and older age group has increased over the last five years (from 31% to 35%). An aging population has implications for the active transportation network as some people may not be able to drive and may desire shorter walking trips to access their destination.

By the numbers...

11,635
year-round residents

10.2%
growth in the number of
Salt Spring Island residents
between 2016 and 2021

56.4
is the median age for the
Island

35%
of the population is 65
years old+

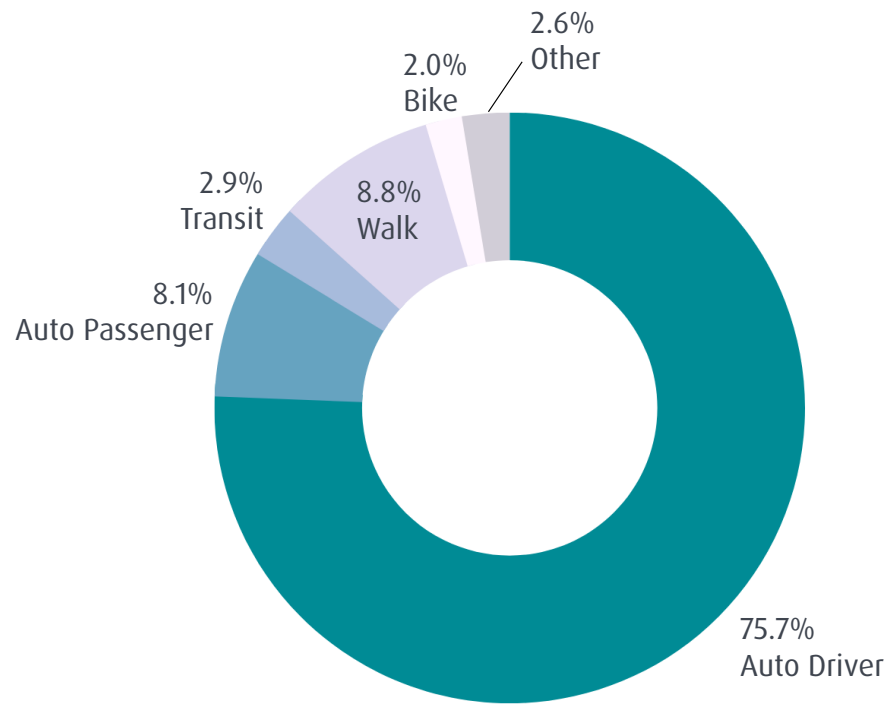
12%
of the population is
between 0-14 years old



2.3 How Salt Spring Islanders Get Around Today

As of 2021, 84% of commuting trips by Salt Spring Island residents were completed as an auto driver or passenger. Walking makes up most of the sustainable commuting mode share (transit, walk, or bike) on the island at 8.8%.

Going beyond commuting trips, the CRD’s 2017 Origin Destination Household Travel Survey provides greater detail into the overall travel patterns on the island. According to the survey, there are approximately 25,000 trips on a typical day on Salt Spring. About 70% of these trips are by auto driver with auto passenger trips comprising 18% of the total mode share, which is likely attributed to the aging population and their dependency on others to drive. Walking makes up most of the sustainable mode share within the district at 6% with cycling and transit accounting for only 2% and 1%, respectively.



Statistics Canada 2021 Journey to Work, Salt Spring Island



By the numbers...

1.69
vehicles per household
(2017)

5.15
daily trips per household
(2017)

84%
driving mode share
(2021)

11%
active transportation
mode share
(2021)

61%
of all commutes are less
than 15 minutes in duration
(2021)

91%
of all commutes stay with-
in Salt Spring Island
(2021)

2.4 Challenges Unique to Salt Spring

As a Gulf Island, Salt Spring has unique challenges that have implications for active transportation planning and that make its community context different from other jurisdictions in the province.

Complex Jurisdictional Framework

Salt Spring Island has a complex jurisdictional framework. As an unincorporated rural Electoral District, it falls under the jurisdiction of the Capital Regional District, which functions as the local government and handles the administrative functions. As an unincorporated area, all roadways on Salt Spring Island are under the jurisdiction of the Ministry of Transportation and Infrastructure (MoTI).

In 1974, the province passed the Islands Trust Act, which established the Islands Trust. As part of the Islands Trust Area, planning for Salt Spring Island must consider and address the unique mandate of the Islands Trust “to preserve and protect the trust area and its unique amenities and environment for the benefit of the residents of the trust area and of British Columbia generally, in cooperation with municipalities, regional districts, improvement districts, other persons and organizations and the government of British Columbia”. The Salt Spring Island Local Trust Area is one of 72 local trust areas and one Island Municipality within the Islands Trust Area. Each Local Trust Committee has been provided with most of the land use planning authority of a regional district board with a mandate, embodied in the “object” of the Islands Trust.

This complex jurisdictional and governance framework can result in more challenges with transportation planning and implementation of new transportation infrastructure.

Seasonal Fluctuations and Ferry Traffic

Salt Spring Island is a popular tourist destination and experiences its peak tourist season between June and September when the island’s population nearly doubles. During this time, many visitors attend the community’s popular Saturday Market in Ganges Village, which features local artisans and food vendors, as well as music and other entertainment. This results in parking challenges within the village and congestion on the island’s rural road network.

Salt Spring Island is primarily accessed by ferry, and communities that rely on ferry transportation to access the mainland face a unique set of transportation challenges. Ferry transportation, particularly during peak travel times, such as holidays or weekends, results in high-volume vehicle traffic arriving at the same time. These spikes in ferry traffic volumes create less safe and comfortable conditions for active transportation users due to the limited road space for people walking and cycling near the ferry terminals.

Existing Active Transportation Facilities Not Meeting AAA Standards

Salt Spring faces challenges in accommodating vehicle traffic on its narrow and at times steep and winding roads while retrofitting the existing right-of-way for active travellers. Most of the roads were built to a ministry standard, which means they are intended to move vehicle traffic efficiently and not intended to serve the needs of those using active transportation.

Outside of Ganges Village, the island is largely rural, hilly, scenic, and with destinations scattered across it. Walkable shoulders with no buffer and or narrow unpaved pathways with minimal protection from vehicle traffic may not feel safe or comfortable for some. The shoulders are also narrow, which often means people walking or biking have to maneuver into the vehicle travel lane in order to pass safely. Further, most of the shoulders have poor pavement conditions such as cracks, potholes, and loose gravel. The existing walking conditions within Ganges Village also vary and there are accessibility challenges for some people walking and rolling. In some cases, this includes insufficient sidewalk widths or curb letdowns, sidewalk obstructions, and unclear crossings that may not be accessible to all or leave some people feeling uncomfortable.

The CRD recently approved a new standard for all ages and abilities facilities, which generally speaks to the need for roads to be more inclusive for all users including those with mobility aids and/or children wanting to walk or roll to school, for example.



2.5 Policy Context

Salt Spring Island has several planning, policy, and technical studies with direct relevance to active transportation that will help shape the direction of the Active Transportation Network Plan.

Official Community Plan (2008)

The Salt Spring Island Official Community Plan (OCP) was adopted on October 8, 2008, providing a long-term vision for the community and guiding principles on future planning, growth, and development. The OCP identifies the following priorities that have relevance to the ATNP:

- Multi-family dwellings that offer safe walking, cycling and transit links to schools, village services and one's that are near villages will be given preference.
- Support for future settlement patterns that reduce automobile dependency and encourage walking, cycling and public transit.
- Village containment boundary policies for Ganges, Fulford and Channel Ridge to keep the development compact.
- Support for multi-modal transportation for inter-village travel.

SSI's OCP recognizes the desire to limit automobile dependence and provides policy direction to integrate walking and cycling within the public transit system.

Pedestrian and Cycling Master Plan (2013)

The Pedestrian and Cycling Master Plan (PCMP): Salt Spring Island Edition recognizes the area's unique character and transportation context. It also outlines the current and desired level of development consistent with the vision used in developing the network for the Regional PCMP and previous bicycle planning efforts on the island. This plan acknowledges transit's integral role in ensuring the success of an active transportation strategy and includes considerations such as:

- Appropriately planning for expected demand
- Providing connections between active transportation and transit networks
- Providing appropriate facilities at transit destinations and stops
- Creating convenient pedestrian and cycle access to transit destinations and stop
- Accommodating a wide range of pedestrians and cyclists in the design of transit destinations and stops

The plan considers the needs of pedestrians and cyclists through the concept of universal design. Its goal is to provide familiar and safe environment for both in urban and rural communities across the region.

Ganges Village Area Plan (Ongoing)

The SSI Local Trust Committee adopted resolutions approving the Project Charter to launch the Ganges Village Area Plan planning process on November 10, 2020. The plan focuses on:

- Supporting the community's social, environmental, economic, and cultural goals
- Tackling the causes and impacts of climate change (net-zero emissions, promoting and integrating sustainable mode share)
- Ensuring that development occurs in a sustainable, coordinated, and resilient manner addressing the issues impacting the social, cultural, and environmental sustainability of Ganges Village

Some of the engagement feedback heard so far through the Ganges Village Area Plan process include:

- The poor condition of sidewalks and crosswalks
- Accessibility concerns, noting that walking is especially hazardous for older people and those with mobility challenges
- The car-centric nature of the village makes it noisy and less attractive/enjoyable

There is strong desire for a more walkable village that could include pedestrian only areas; traffic calming; wider and more accessible sidewalks; and cycling facilities.

Salt Spring Elementary Active School Travel Report (2020/21)

Salt Spring Elementary School participated in a school catchment / neighbourhood-based planning process with the aim to increase rates of students walking and rolling to/from school. Located in Ganges Village, the school's catchment area also extends to serve students living in Long Harbour, Fulford and Ganges. Key findings of the baseline survey and report include:

- 30% of students used active travel to get to and from school or the school bus stop
- 70% of families chose to travel by car or carpool to and from school or the bus stop
- Almost 10% of students were driven to and from school but live within 1.5km of Salt Spring Elementary
- Of all the Salt Spring Elementary families who usually drive, 82% of families would prefer walk & wheel, but choose to drive because of traffic safety concerns & convenience/time pressure

The Salt Spring Active School Travel Report highlights the unrealized potential of converting vehicle trips related to school arrival and dismissal. The report identifies several tips and strategies for families to encourage them to walk and bike to school. However, what is needed is new active transportation infrastructure that is safe, comfortable, and connected to increase the share of walking and cycling to/from school.

Salt Spring Island Transit Future Service Plan (2021)

The Salt Spring Island Transit System has seen ridership grow from 100,000 riders in 2014/15 to 116,000 riders in 2019/20. The key objectives for Salt Spring Island include the electrification of their transit fleet and better integrating transit service with active modes of transportation and ferry service.

Two additional bays are needed within the existing bus stop in Ganges Village for the proposed increase of peak service. This will result in the removal of on-street parking along Lower Ganges Road in front of the Salt Spring Visitor Information Centre.

BC Active Transportation Design Guide (2019)

The BC Active Transportation Design Guide (BC ATDG) was released in June 2019. The guide is a comprehensive set of planning and engineering guidelines offering recommendations for the planning, selection, design, implementation, and maintenance of active transportation facilities across the province. It contains engineering principles and best practices from the municipal, provincial, national, and international levels. These two efforts call for more protected cycling facilities generally using motor vehicle speeds and volumes as the primary way to determine what sort of facility should be provided.

As the ATNP was funded from the BC Active Transportation Infrastructure Grants Program, all of the recommended active transportation facilities in this plan must align with the BC ATDG.

3.0 Public & Stakeholder Engagement

The ATNP included a robust public and stakeholder engagement process to ensure that the final recommendations reflected the needs and priorities of island residents, particularly those living in proximity to Ganges Village. Over the course of the plan process, the community was invited to participate in two rounds of engagement. Each of the engagement activities were promoted using several tools, including the project webpage (Get Involved), social media platforms, and emails to stakeholders.

The engagement level – as defined by the International Association of Public Participation (IAP2) Framework’s and included in CRD’s Public Participation Framework was “involve”. This means that the project team will “work with the public to ensure concerns and aspirations are understood and considered”. The CRD commits to working “with the public to exchange information, ideas, and concerns”.

A summary of the engagement activities included in the ATNP is provided below. For a detailed summary of the engagement feedback from the first round, please see the Salt Spring Island ATNP Phase 2 Engagement Summary Report.

Across the two rounds of engagement for the ATNP process, over 500 participants shared their input...

252
online survey #1
respondents

155+
ideas fair
participants



20
interactive mapping activity
participants



14
residents provided their
input through an ASK Salt
Spring conversation



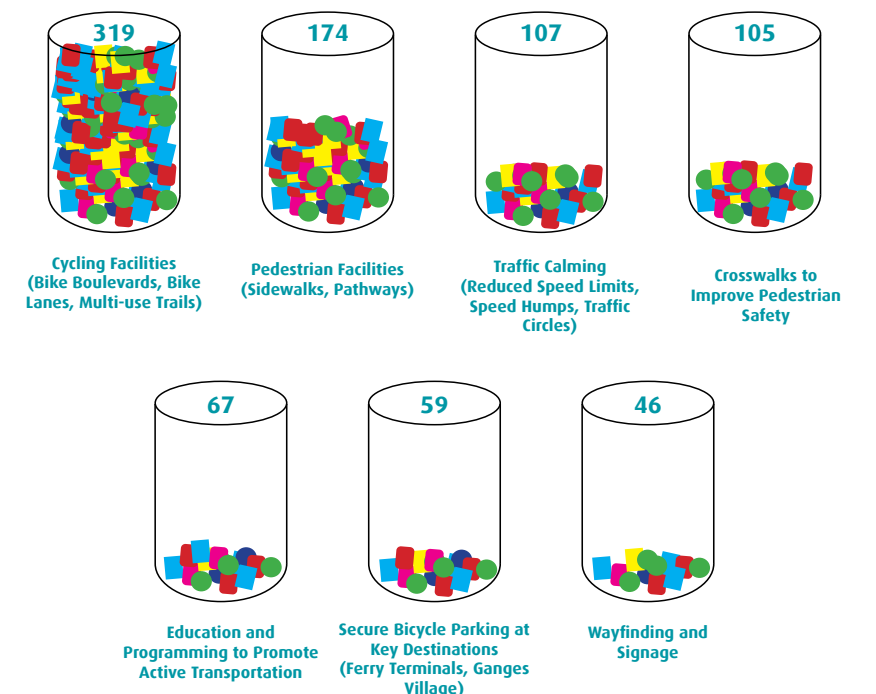
13
in-person stakeholder
workshop participants

82
online survey #2
respondents



Ideas Fair

Over 150 people offered their ideas, experiences, and perspectives at an in-person Ideas Fair, identifying barriers they face in accessing Salt Spring Island’s active transportation network and opportunities they would like to see explored in the plan. The event also included an activity called “planner for the day” where participants were provided with six tokens and asked to place them in an active transportation improvement category based on their top priorities. ‘Cycling facilities’ was the top priority (319 tokens) followed by pedestrian facilities with 174 tokens.



Online Survey #1

To better understand the existing conditions of Salt Spring's active transportation network, an online survey was open to the public on the CRD's Get Involved platform from October 3rd to 31st, 2022. A total of 252 surveys were submitted where Salt Spring residents and visitors shared their experiences, preferences, and ideas for the future of active transportation on the island. An interactive mapping activity was also available on the CRD's Get Involved platform. Participants were asked to identify barriers that they experience as a person walking, rolling, or cycling around Salt Spring Island. 20 participants provided locations on the map, identifying opportunities for improvements, including locations for new bike lanes, better maintenance of existing paths, and unsafe intersections.

ASK Salt Spring

On October 28th, 2022, a group of 14 residents joined CRD's Transportation Manager for an ASK Salt Spring conversation. Through this discussion, participants learned about the Active Transportation Network Plan process and shared their concerns, experiences, and ideas for moving around the island. Participants also shared their concerns and solutions to enhance pedestrian and cycling safety in the village, including changing/adding crosswalks, adding stop signs at Ganges intersections, mini-roundabouts, and reduced (and enforced) speed limits.

Salt Spring Island Transportation Commission Presentation

On November 28th, 2022, the project team presented a summary of the first two phases of the ATNP project to the Salt Spring Island Transportation Commission. This included a summary of the baseline conditions report and the first round of engagement. Committee members had an opportunity to ask questions and provide feedback on the work to date and the specific items they would like to see further explored in the latter phases of the ATNP.



Stakeholder Workshop

An in-person stakeholder workshop was undertaken on February 7th, 2023 at Salt Spring's Public Library. The three-hour workshop provided an opportunity for a detailed discussion on the proposed active transportation improvements within Ganges Village. The project team provided a project overview of the work undertaken to date and presented the proposed corridor improvements. Facilitators led the group through a discussion around the various tradeoffs associated with the proposed improvements and gathered feedback on their level of support and preferences. Several cross-section options were presented for each corridor that were being explored in Ganges Village. Most were refined and modified based on feedback from the group while some were eliminated entirely based on consensus from the participants.

Of the 17 stakeholders invited, the following stakeholders were in attendance:

- BC Transit
- Chamber of Commerce
- Country Grocer
- Cycling Salt Spring
- Island Pathways
- Islands Trust (attended as an observer)
- Ministry of Transportation and Infrastructure
- Salt Spring Island Housing Council Society
- Salt Spring Island Transportation Commission (attended as an observer)
- Salt Spring Seniors Centre
- Salt Spring Solutions
- Salt Spring Library
- Windsor Plywood

Online Survey #2

To better inform the final list of recommendations for the ATNP, a second online survey was open to the public on the CRD's Get Involved platform from March 3rd to 24th, 2023. A total of 82 surveys were submitted where participants provided feedback and preferences on the draft network and infrastructure improvements.

4.0 Plan Framework

4.1 Vision

A vision statement was created for the Active Transportation Network Plan to capture what Ganges Village aspires to be in 10 years from now. The vision statement was informed by the public engagement undertaken as part of the ATNP and reflects the broader transportation themes from the Ganges Village Local Area Plan process.

Ganges Village is a place to gather, where the community—including residents, visitors, and those employed in the local economy—can move around the village seamlessly, safely, and actively by any mode of active transportation. The abundance of communal gathering spaces reflects the vibrancy and livability of Ganges Village. The active transportation network has been transformed, where a larger share of the roads has been reallocated to people walking, rolling, and cycling. All destinations within the village are easily accessible for all users, especially those with mobility challenges. Ganges Village is connected to the larger island, accessible, and safe for all where active transportation is the preferred way to move around — lowering the community’s GHG emissions, contributing to the local economy, and allowing Ganges to realize its full potential as a complete community and as a cultural tourism destination.

4.2 Guiding Principles

Five guiding principles were developed for the Active Transportation Network Plan. They reflect the values of active transportation planning on Salt Spring and serve as the framework for the recommendations in this plan.

1 Low-cost, Quick-build, & Actionable Solutions

A quick-build approach is always explored first for all active transportation improvements to minimize costs and build new projects in a time efficient manner.

2 A Pedestrian Friendly Village

Ganges Village is a pedestrian friendly village that maximizes space for people. The Village is designed in a way that maximizes safety and comfort for those walking and rolling while retaining on-street parking where possible.

3 Integration & Connectivity

Ganges Village is part of the island’s larger active transportation network that enables safe and convenient connections to key community destinations.

4 A More Accessible Ganges

An active transportation environment that enables residents and visitors of all abilities to walk and roll safely, especially those with mobility aids or strollers.

5 Active Transportation Solutions are Climate Solutions

Consistent with the CRD’s Regional Climate Action Strategy, new active transportation infrastructure can mitigate climate impacts where on-road transportation represents the largest source of the region’s greenhouse gas (GHG) emissions. All active transportation solutions will be positioned, framed, and articulated in the larger context of meeting the CRD’s commitment to reduce GHG emissions by 40% by 2030.

What We Heard

In the second survey, we asked the community *“What is your level of support for draft guiding principles?”*

Over 89% of survey respondents indicated they strongly support or support the guiding principles.

Guiding principle #5 (Active Transportation Solutions are Climate Solutions) was added based on feedback heard at the stakeholder workshop.

4.3 Strategy Areas

The ATNP includes three strategy areas that provide specific direction on how active transportation improvements can be realized within Ganges Village. The strategy areas build on the guiding principles above and contain recommended actions that were informed by the public engagement process and technical analysis undertaken as part of developing the ATNP.

Strategy Area 1 Reallocate Street Space for Walking, Rolling, and Cycling

Action 1A:
Implement McPhillips Avenue
pedestrian facilities

Action 1B:
Implement Rainbow Road pedestrian
and cycling facilities

Action 1C:
Implement Fulford-Ganges Road
pedestrian and cycling facilities

Action 1D:
Implement Lower Ganges Road
pedestrian and cycling facilities

Action 1E:
Adopt infrastructure design guidelines

Action 1F:
Connect to active transportation
network outside of Ganges Village

Strategy Area 2 Make Ganges Village Accessible for all Residents and Visitors

Action 2A:
Conduct a land ownership survey of the
Ganges Village

Action 2B:
Adopt a 30 km/h posted speed limit in
the core of Ganges Village

Action 2C:
Create village gateways with traffic
calming design interventions

Action 2D:
Undertake reviews of intersections
within Ganges Village

Action 2E:
Retain accessible parking within the
Village

Action 2F:
Establish no-parking zones with
physical barriers where necessary

Strategy Area 3 Encourage Sustainable Modes

Action 3A:
Develop a bike parking retrofit program

Action 3B:
Identify opportunities to increase off-
street parking

Action 3C:
Develop a parking wayfinding program

Action 3D:
Work with BC Transit to integrate active
transportation modes

Big Move

4.4 Key Infrastructure Facility Type Definitions

The following infrastructure types are frequently referenced throughout this document. Expanded definitions can be found in Section 5.0 'Action 1E'.

Key Definition: Adaptive Sidewalk

Adaptive sidewalks are often used where there is no existing sidewalk or where it may be difficult to construct a raised sidewalk. They typically include a barrier from the roadway.



Key Definition: Multi-use Pathways

Multi-use pathways (MUPs) are typically off-street pathways that are separated from motor vehicle traffic and can be used by any active transportation user.



Key Definition: Protected Bicycle Lane

A designated lane for people cycling and other active transportation users that is physically separated from motor vehicle traffic and people walking and rolling.



Key Definition: Painted Bicycle Lane

Separate travel lanes designated exclusively for bicycle use that are delineated by a painted line and, in some cases, a painted buffer area.



5.0 Strategy Area 1

Reallocate Street Space for Walking, Rolling and Cycling

Strategy area 1 is focused on roadway reallocation to prioritize the needs of people cycling, walking, and rolling. A total of 11 actions are recommended to mitigate the gaps in the active transportation network and address the feedback heard through the public engagement process where participants identified the lack of shoulders and insufficient road space as the main barriers to walking and cycling on the island and within Ganges Village. The engagement process also confirmed that providing more separation and protection from vehicles along corridors is among the top priority for residents to improve their active transportation experience and address road safety concerns.

In addition to the public engagement process, transportation data indicate the need for greater separation of vehicles and active transportation modes as evidenced by the following:

- Currently within Ganges Village, there are limited pedestrian and cycling facilities. Sidewalks are provided on Lower Ganges Road, Rainbow Road, and other select blocks. A painted bicycle lane is also available for part of Lower Ganges Road. Except for 95 metres of separated sidewalk on Fulford-Ganges Road, none of these facilities meet the definition of all ages and abilities per the BC Active Transportation Design Guide.
- According to BC Ferries, there were a total of 15,756 bicycle arrivals at Fulford, Vesuvius Bay and Long Harbour between April 2021 to September 2021, which represents the busier tourist season. This number increased to 17,879 bicycle arrivals in 2022 for the same months, or a 13% increase. Even though the data do not indicate which roads people cycling take once they arrive on the island, many do cycle through Ganges Village via Fulford-Ganges Road and Lower Ganges Road where cycling facilities are not present and/or not protected from motor vehicles.
- According to MOTI, based on a July 2022 traffic count the motor vehicle volumes on Fulford-Ganges Road are in the range of 2,950-3,050 vehicles per day, with nearly 7 to 10% as heavy vehicles. And for some segments of the road, including within Ganges Village, the average AM vehicle speeds were 10% or more above the posted speed limit. According to the BC Active Transportation Design Guide, in rural contexts, traffic volumes above 2,500 vehicles a day justify a bicycle lane (bicycle accessible shoulder in the BC Active Transportation Design Guide).
- The Strava data indicate high cycling activity along Fulford-Ganges Road and Lower Ganges Road. Strava is a mobile app tracking human exercise and incorporates social network features. It is mostly used for cycling and running using GPS data.

Based on the engagement results and technical data, several corridor improvements are recommended and detailed in the following actions. The corridor improvements, once implemented, would result in 1.9 kilometres of new active transportation facilities. Conceptual cross-sections are included in this section to illustrate the recommended active transportation corridors.

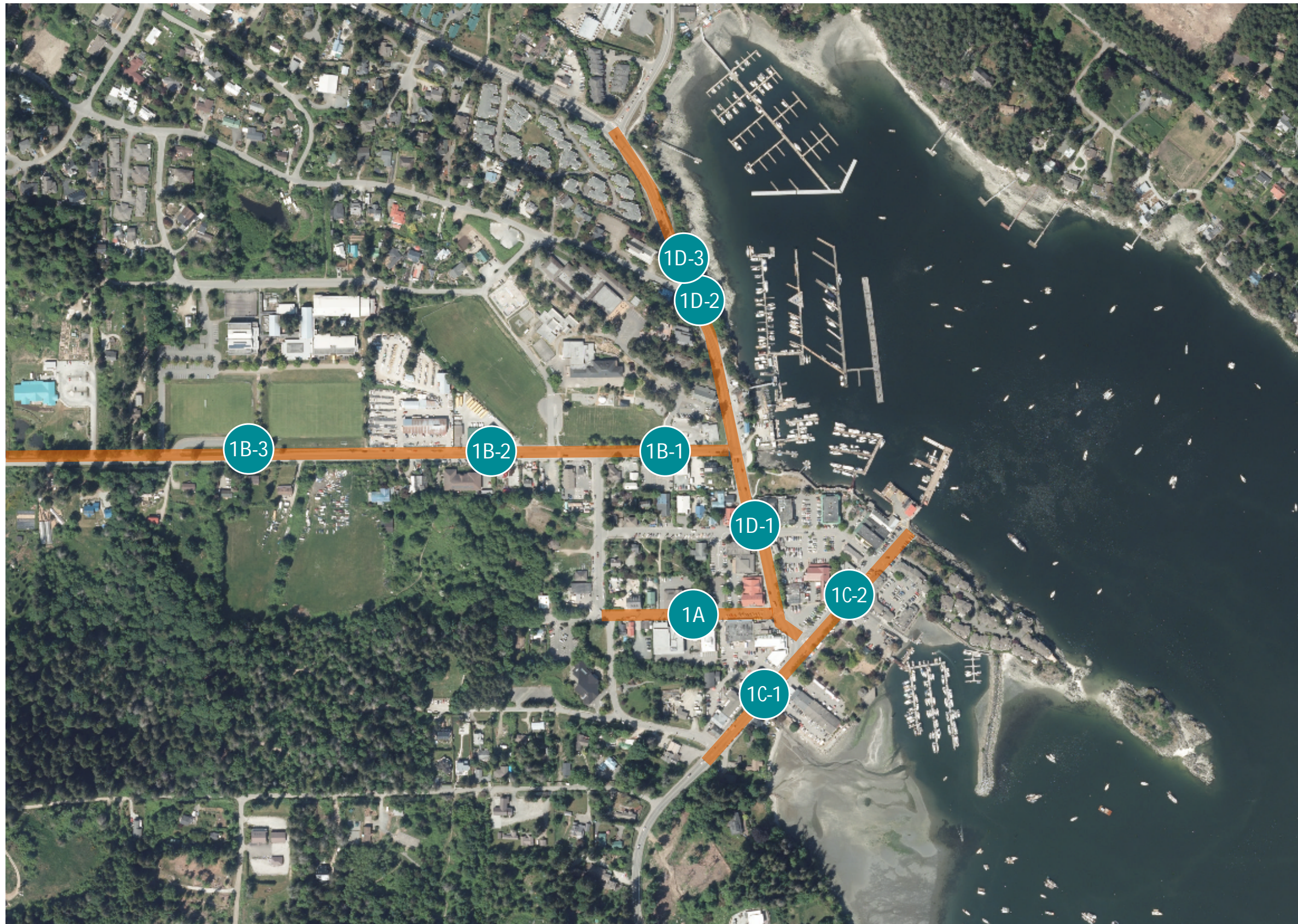
What We Heard

"If there was a proper bike lane - doesn't even need to be too wide - particularly along Fulford-Ganges road, I would be much more inclined to use my e-bike to ride into Ganges. As it is now, it is a treacherous experience."

"Vehicles, bikes and people do not share lanes well. Separation is needed."

"A walkable island that encourages active forms of transportation. Bike/walking paths throughout the island especially leading into Ganges. A layout in Ganges that makes walking the most efficient way to get around. Generally a feeling that walking and biking is safe. As it is now we hesitate to do it as it often is stressful especially with a young child. I didn't think we would move to such a beautiful place and end up driving more than we did in the city."

"Transport is built with cycling, pedestrians, and those with mobility issues first: and cars second. A place where an 80 year old and an 8 year old can travel safely on foot/bike, without fear."



Priority Corridors

- 1A McPhillips Avenue**
From Jackson Ave to Lower Ganges Rd
- 1B-1 Rainbow Road**
From Lower Ganges Rd to Jackson Ave
- 1B-2 Rainbow Road**
From Kanaka Rd to Windsor Pylwood
- 1B-3 Rainbow Road**
West of Windsor Plywood
- 1C-1 Fulford-Ganges Road**
From Seaview Ave to Lower Ganges Rd
- 1C-2 Fulford-Ganges Road**
From Lower Ganges Rd to Purvis Lane
- 1D-1 Lower Ganges Road**
From Fulford-Ganges Rd to Rainbow Rd
- 1D-2 Lower Ganges Road**
From Rainbow Rd to Upper Ganges Rd
(Interim Solution)
- 1D-3 Lower Ganges Road**
From Rainbow Rd to Upper Ganges Rd
(Ultimate Solution)

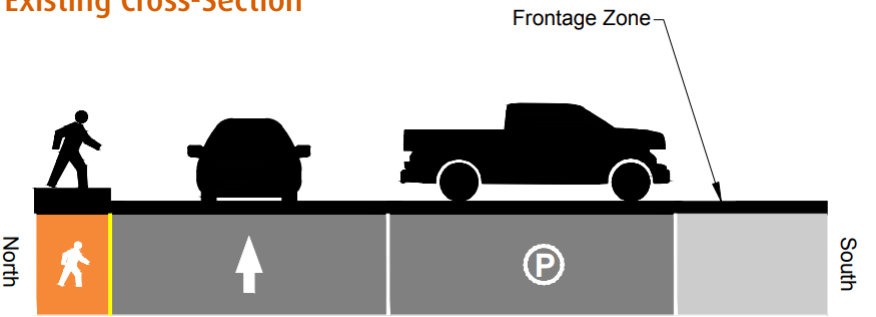
Action 1A Implement McPhillips Avenue Pedestrian Facilities

McPhillips Avenue is a critical east-west connection within Ganges Village. It has a sidewalk on the north side and limited pedestrian facilities on the south side. Based on the 2018 Ganges Village Parking Study, there are a total of 31 formal on-street parking spaces with different configurations including parallel, perpendicular, and angled. People also park informally along this corridor to get closer to their destinations.

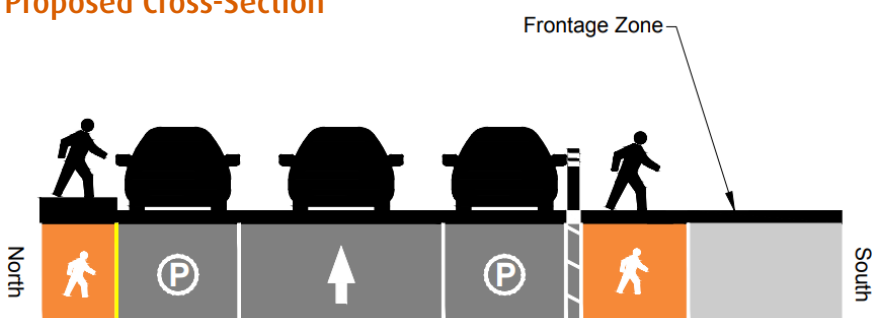
The proposed cross-section is shown below and is summarized as follows:

- The existing vehicle travel lane would be reduced from 5.5 metres to 4 metres.
- The existing angled and perpendicular parking along the south side would be converted to parallel parking to accommodate an adaptive sidewalk along the frontage. Adaptive sidewalks are a facility type that have been piloted and implemented in various jurisdictions. They are often used where there is no existing sidewalk or where it may be more challenging to construct a raised concrete sidewalk due to physical or budgetary constraints. This would result in the loss of approximately 6 spaces.
- Removing angled parking reduces potential conflicts with active transportation users and allows for the narrowing of the travel lane, which helps facilitate lower vehicle speeds.

Existing Cross-Section



Proposed Cross-Section



What We Heard

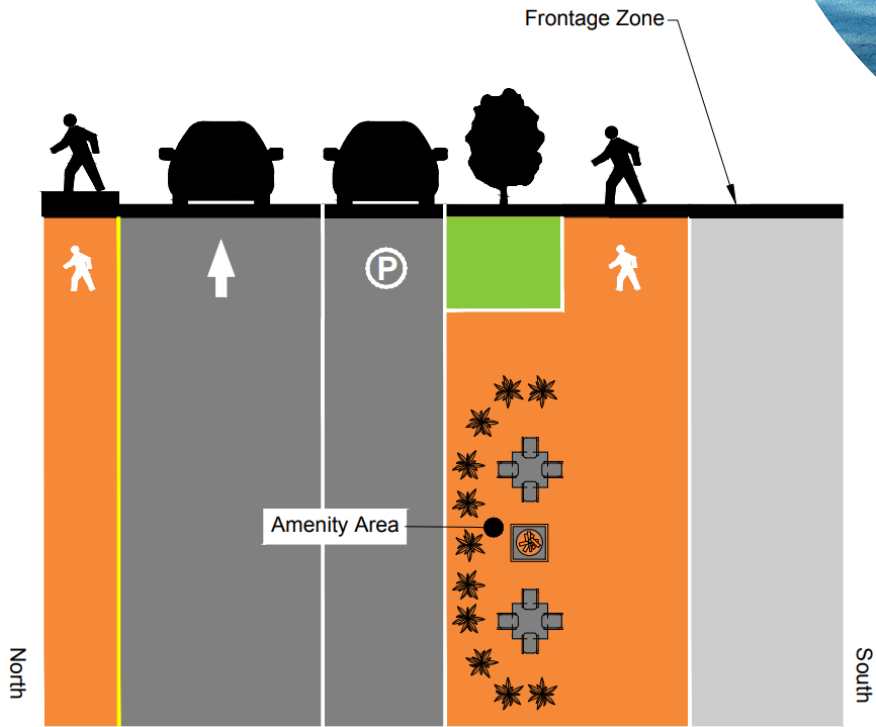
In the second survey, we asked the community “What is your level of support for seeing new pedestrian facilities on McPhillips Avenue?”

Over 81% of survey respondents indicated they strongly support or support the proposed cross-section.

The proposed corridor improvements to McPhillips Avenue are anticipated to improve pedestrian safety and increase overall walkability within the village. To further pedestrianize the corridor and to help meet the guiding principle of a “pedestrian friendly village”, the CRD could work with MOTI to create an amenity area along the south side of the road in the summer months (i.e., tourism high season) for a select number of days or weeks. The cross-section below illustrates what this seasonal treatment could look like. There would be a loss of approximately 15 parking stalls in exchange for a public amenity area that provides an opportunity for residents and visitors to gather, rest, and socialize—all of which is in line with the guiding principle of creating a pedestrian friendly village.

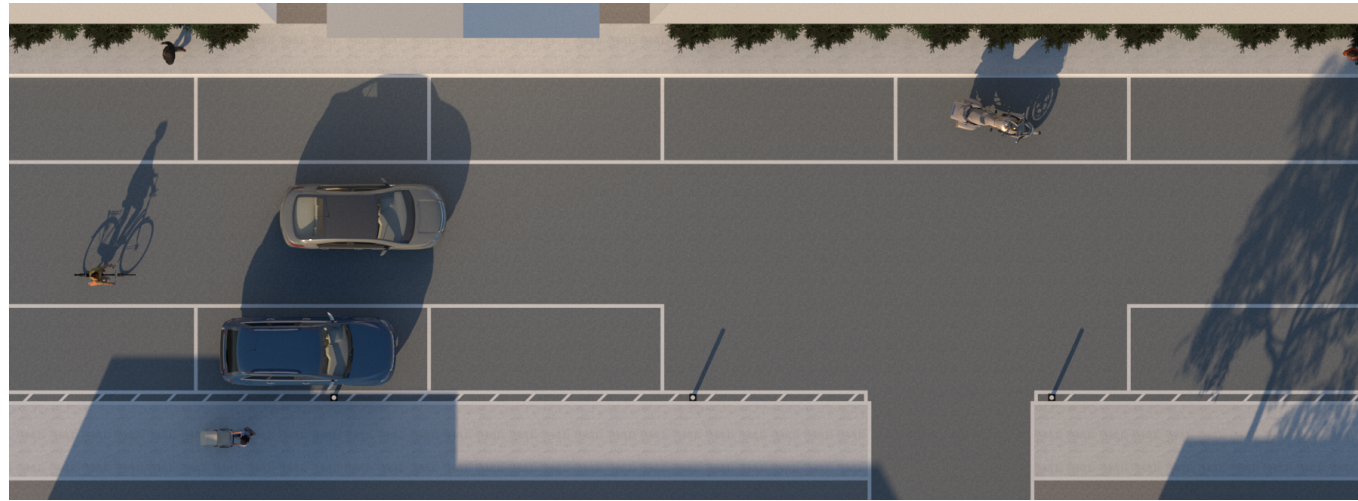
Further, the proposed improvements to McPhillips Avenue could be completed simultaneously with the new off-street parking lots recommended at ArtSpring (see Action 3B).

Proposed Cross-Section for Seasonal Activation



1A McPhillips Avenue

Option 1 - Improved Pedestrian Facilities



Sidewalk	Parallel Parking	Travel Lane	Parallel Parking	Sidewalk
----------	------------------	-------------	------------------	----------

*Conceptual images for indication only

1A McPhillips Avenue

Option 2 - Pedestrian Facilities + Planting & Amenity Zone



Sidewalk	Travel Lane	Parallel Parking	Planting & Amenity Zone	Sidewalk
----------	-------------	------------------	-------------------------	----------

*Conceptual images for indication only

Action 1B Implement Rainbow Road Pedestrian and Cycling Facilities

Rainbow Road serves as the main east-west corridor within Ganges Village and provides connections to other parts of Salt Spring Island.

The existing cross-section is shown below and is summarized as follows:

- Between Jackson Road to Lower Ganges Road, there is an existing concrete curb sidewalk on the north side and a shoulder on the south side where informal parking occurs.
- The sidewalk transitions to an at-grade gravel pathway along the north side of Rainbow Road west of Jackson Road until Windsor Plywood where a combination of parallel and perpendicular on-street parking is available.
- West of Windsor Plywood, the pedestrian facility continues as an at-grade gravel pathway with a ditch separating the facility from the motor vehicle travel lane.

Given the length of the Rainbow Road corridor, the proposed corridor improvements are presented in three segments in the following sections. It is recommended that improvements to all three segments occur at the same time.



1B-1 From Jackson Road to Lower Ganges Road

Rainbow Road serves as the main east-west corridor within Ganges Village and provides connections to other parts of Salt Spring Island.

The proposed cross-section for Jackson Road to Lower Ganges Road and is summarized as follows:

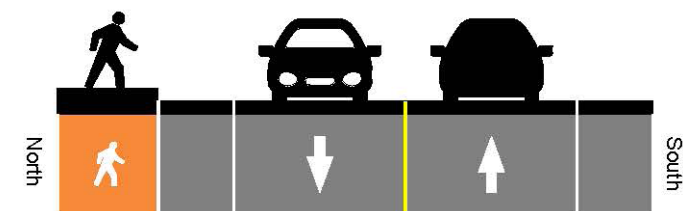
- The existing concrete curb sidewalk on the north side is retained.
- The shoulder on the north side is formalized into a painted bicycle lane (with pavement markings and signage).
- The existing shoulder on the south side of the road is widened to accommodate an adaptive sidewalk / pathway with flexible delineator posts or a rubber curb. The facility, recommended to be 2.0m in width, would be intended to serve people walking, rolling, and cycling. Even though there may be some user conflicts in the busier tourism season, people cycling could choose to take the road for this short stretch to access Lower Ganges Road.
- Even though parking is not allowed on the south side of the road, the recommended changes would prevent vehicles from continuing to park there in the future. The existing uses on the south side are mixed including a mix of commercial and institutional. A new pedestrian facility will make it easier for people walking and rolling to access these destinations.

What We Heard

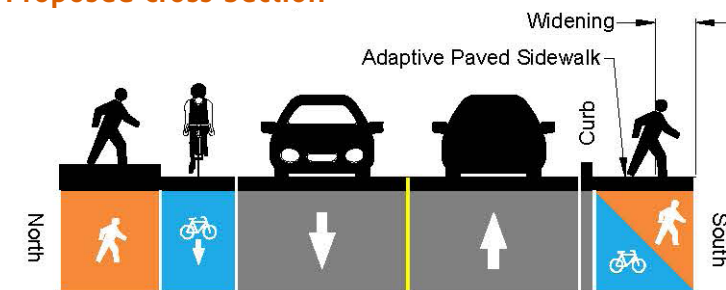
In the second survey, we asked the community *“What is your level of support for seeing new pedestrian and cycling facilities on Rainbow Road?”*

Over 88% of survey respondents indicated they strongly support or support the proposed cross-section from Jackson Road to Lower Ganges Road.

Existing Cross-Section



Proposed Cross-Section



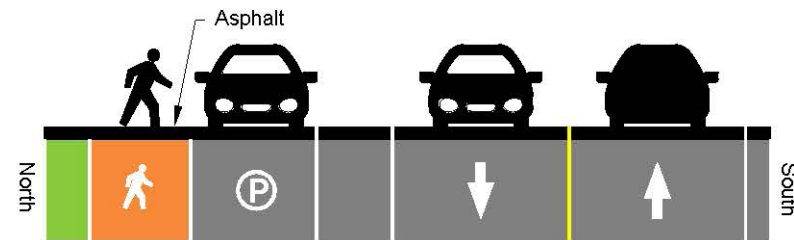
1B-2

From Kanaka Road to Windsor Plywood

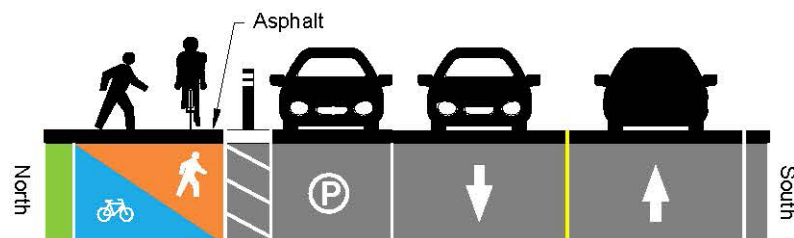
The proposed cross-section for Kanaka Road to Windsor Plywood and is summarized as follows:

- The existing gravel path is widened to a multi-use pathway to be shared by those walking, rolling, and cycling to prevent informal parking.
- A buffer with bollards and/or flexible delineator posts is provided along the Windsor Plywood frontage to provide additional separation from the parking lane. No separation is provided along the SD64 Maintenance Centre due to limited right-of-way.
- There are no anticipated impacts to on-street parking; therefore, the parking supply would remain as is.
- To help mitigate user conflicts on the multi-use pathway, the CRD could install signage and pavement markings to indicate clearly that the pathway is to be shared among different active mode users.

Existing Cross-Section



Proposed Cross-Section

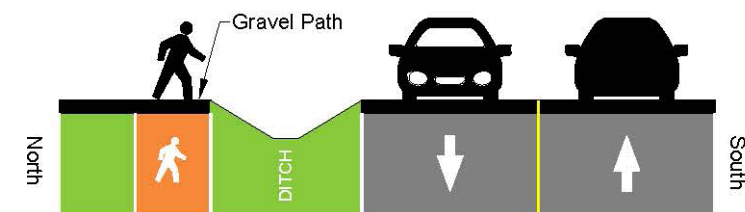


1B-3

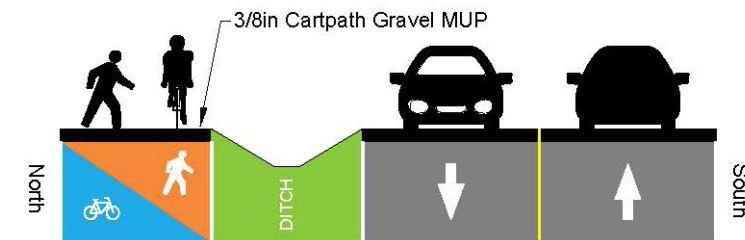
West of Windsor Plywood

As shown below, the proposed cross-section west of Windsor Plywood is to widen the existing gravel path into a wider multi-use pathway to be shared by those walking, rolling, and cycling.

Existing Cross-Section



Proposed Cross-Section



Action 1C Implement Fulford-Ganges Road Pedestrian and Cycling Facilities

Fulford-Ganges Road serves as one of the critical gateways into Ganges Village. According to MOTI, the motor vehicle volumes on Fulford-Ganges Road are in the range of 2,950-3,050 vehicles per day based on a July 2022 count including nearly 7 to 10% as heavy vehicles. And for some segments of the road, including within Ganges Village, the average AM vehicle speeds were 10% or more above the posted speed limit. Based on these data, protected pedestrian and cycling facilities are required for this corridor to improve road safety for all users and to increase overall comfort for active transportation users.

The existing cross-section for this corridor varies depending on the location; however, the area of focus within Ganges Village is from Seaview Avenue to Lower Ganges Road and Lower Ganges Road to Purvis Lane.

- **Seaview Avenue to Lower Ganges Road:**
The existing cross-section for this segment includes a separated sidewalk with parallel parking along the north side of the road. The south side includes parallel parking with no pedestrian or cycling facilities.
- **Lower Ganges Road to Purvis Lane:**
There is currently angled parking on both sides of the road. There is a sidewalk on the north side and a pedestrian pathway on the south side.

What We Heard

In the second survey, we asked the community *“What is your level of support for new pedestrian and cycling facilities on Fulford - Ganges Road?”*

Over 78% of survey respondents indicated they strongly support or support the proposed cross-section from **Seaview Avenue to Lower Ganges Road**.

Over 70% of survey respondents indicated they strongly support or support the proposed cross-section from **Lower Ganges Road to Purvis Lane**.

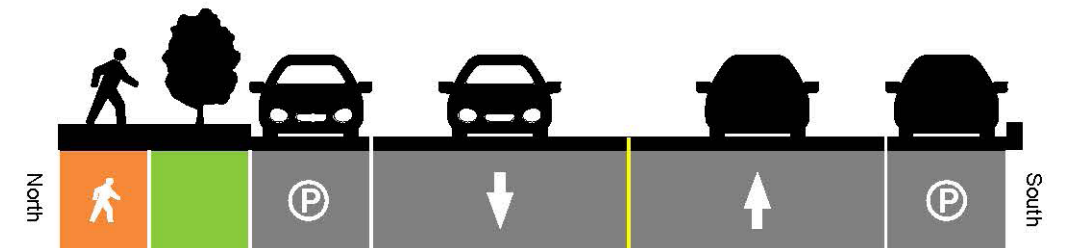
“Vehicles, bikes and people do not share lanes well. Separation is needed.”

1C-1 From Seaview Avenue to Lower Ganges Road

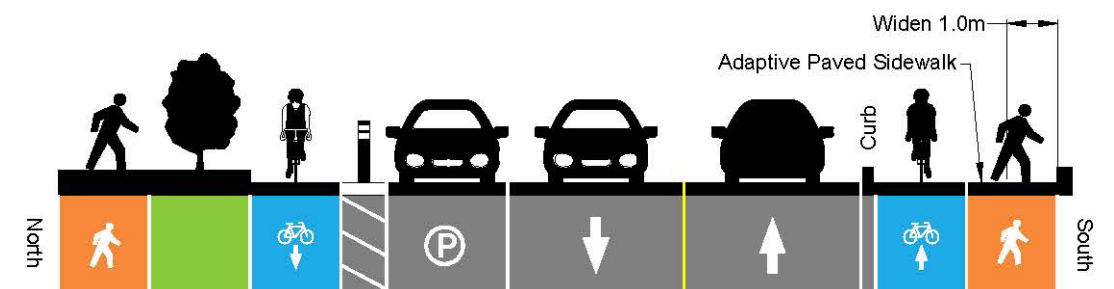
The proposed cross-section for Seaview Avenue to Lower Ganges Road is shown below and is summarized as follows:

- The existing concrete curb sidewalk on the north side is retained.
- The existing vehicle travel lanes would be reduced from 5.1m to 3.5m.
- The existing on-street parking on the north side is retained.
- A protected bicycle lane is provided along the north side of the road separated by bollards and/or flexible delineator posts.
- An adaptive sidewalk and protected bicycle lane is provided on the south side, which results in the loss of nine on-street parking spaces.
- A pedestrian refuge island and crosswalk is added at Seaview Avenue to slow vehicles down and serve as one of the “gateways” into the village to signal to drivers that they are entering an area with higher pedestrian activity.
- Lastly, the improvements and the new active transportation facilities would also connect with the larger improvements recommended to Fulford-Ganges Road in the Salt Spring Island Cycling Safety Review.

Existing Cross-Section

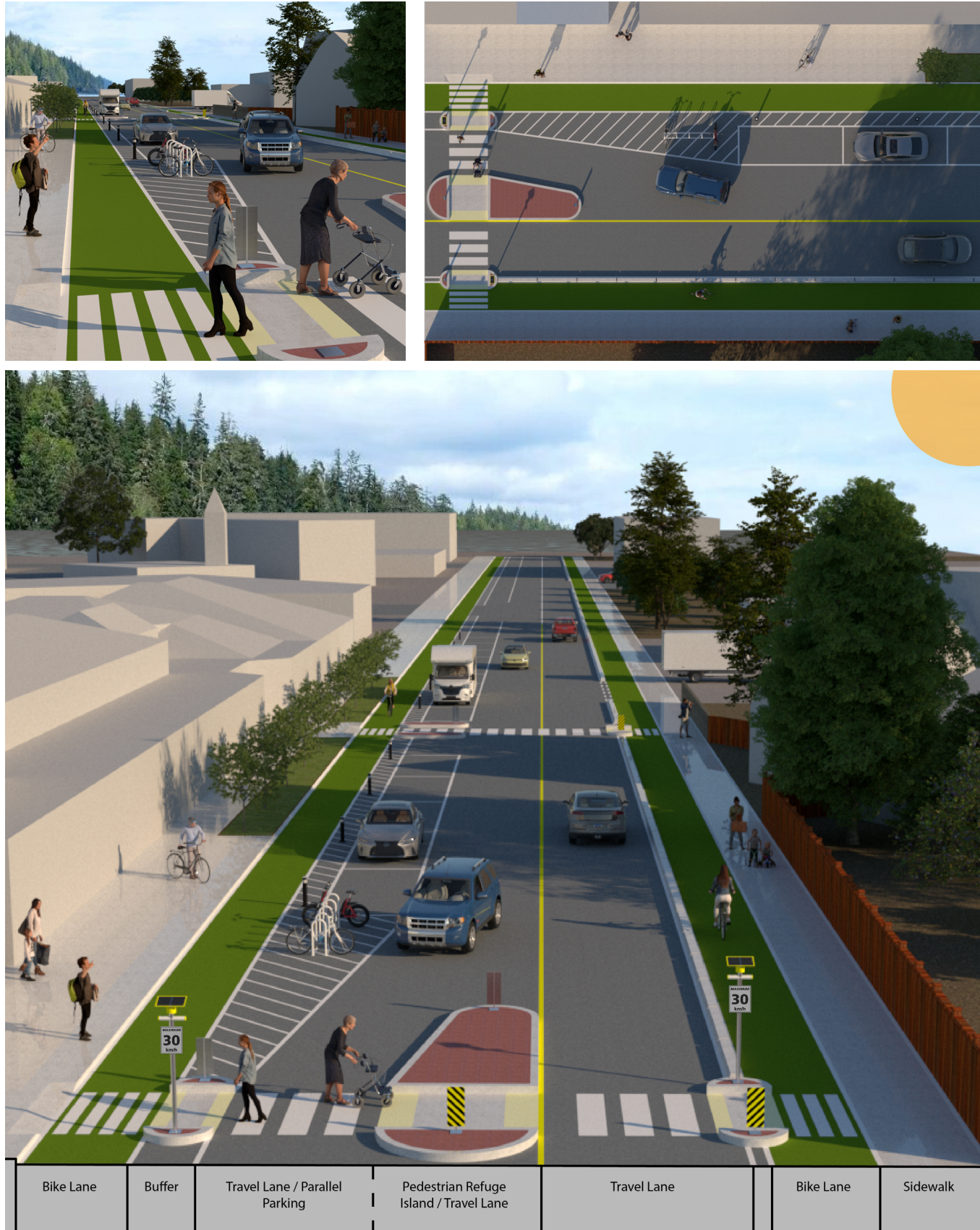


Proposed Cross-Section



1C-1 Fulford-Ganges Road

Improved Pedestrian & Cycling Facilities



*Conceptual images for indication only

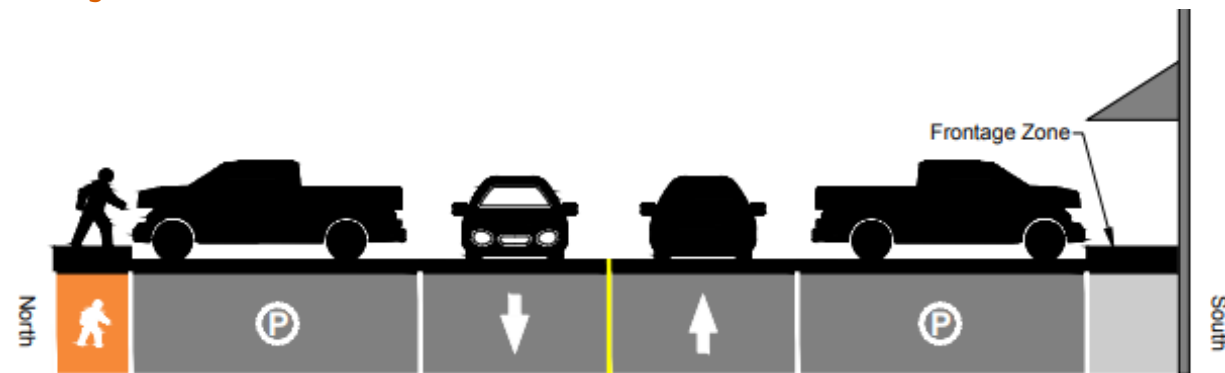
This Page Intentionally Left Blank

From Lower Ganges Road to Purvis Lane

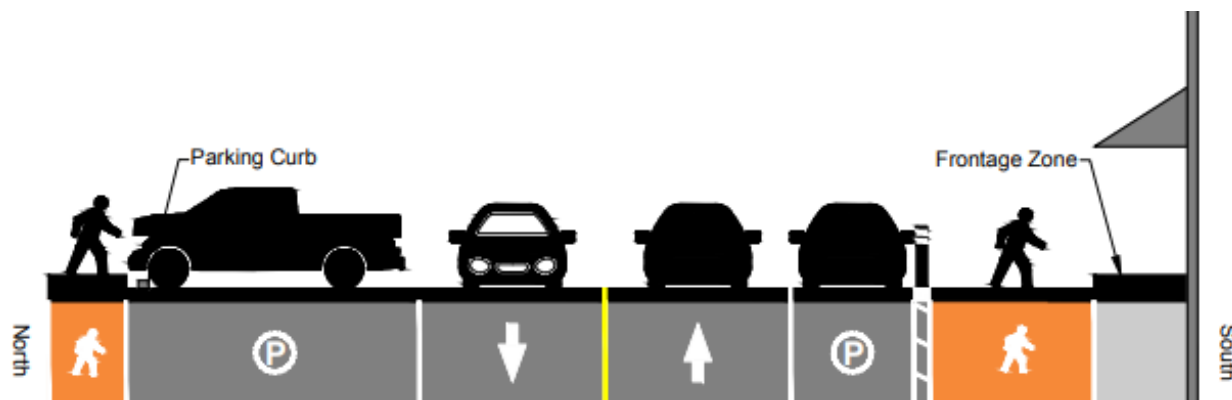
The proposed cross-section for Lower Ganges Road to Purvis Lane is shown below and is summarized as follows:

- The existing vehicle travel lanes would be reduced from 3.7m to 3.5m.
- The angled parking along the south side is converted to parallel parking to accommodate an adaptive sidewalk. This would provide more space for people walking and rolling. This would result in the loss of approximately 7 parking stalls on the south side.
- Wheel stops (parking curbs) are provided consistently in the angled parking spaces along the north side of the road to prevent parked vehicles from encroaching on the sidewalk.

Existing Cross-Section



Proposed Cross-Section



Action 1D Implement Lower Ganges Road Pedestrian and Cycling Facilities

Like Fulford-Ganges Road, Lower Ganges Road serves as the other critical gateway into Ganges Village. The existing cross-section for this corridor varies depending on the location; however, the area of focus within Ganges Village is from Fulford-Ganges Road to Rainbow Road and Rainbow Road to Upper Ganges Road.

- **Fulford-Ganges Road to Rainbow Road:**
This section of the corridor has existing pedestrian facilities (concrete sidewalks) on both sides with limited on-street parking and a bus stop on the east side of the road. There are no cycling facilities provided. There is also a left-turn lane to facilitate turning movements to the east-west streets.
- **Rainbow Road to Upper Ganges Road:**
This section of the corridor has an existing pedestrian facility (concrete sidewalk) and a painted bicycle lane on the west side of the road. The right-of-way varies considerably along the east side of the road with a shoulder, on-street parking for the Marina, and informal parking in some locations.

What We Heard

In the second survey, we asked the community *“What is your level of support for new pedestrian and cycling facilities on Lower Ganges Road?”*

Over 68% of survey respondents indicated they strongly support or support the proposed cross-section from **Fulford-Ganges Road to Rainbow Road**.

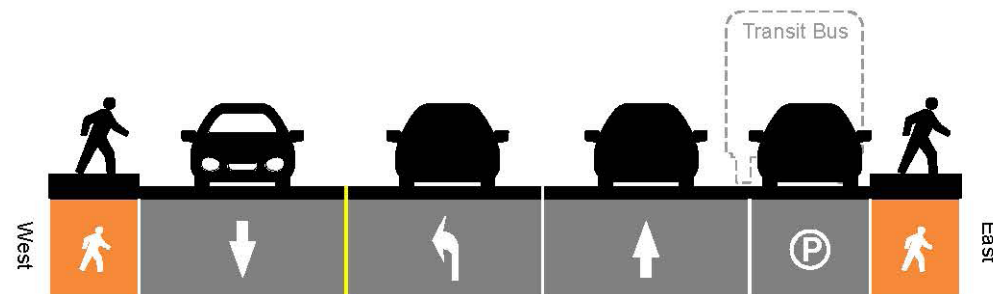
Over 84% of survey respondents indicated they strongly support or support the proposed cross-section from **Rainbow Road to Upper Ganges Road**.

1D-1 From Fulford-Ganges Road to Rainbow Road

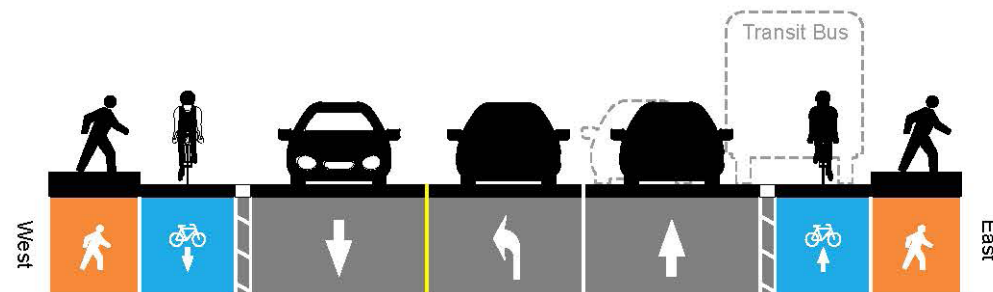
The proposed cross-section for Fulford-Ganges Road to Rainbow Road is shown below and is summarized as follows:

- The existing left-turn lane is retained to minimize impacts to traffic operations within Ganges Village and to the larger road network.
- The existing vehicle travel lanes would be reduced from 4.1m to 3.5m and the left-turn lane would be reduced from 3.9m to 3.1m.
- Buffered bicycle lanes are provided on both sides of the road. This results in the loss of approximately nine on-street parking spaces on the east side of the road.
- The existing transit stop is located within the bike lane and the person cycling will need to maneuver around the bus when it is stationed at the stop.
- Concrete curb sidewalks are constructed on both sides to fill in gaps and provide continuous facility coverage.

Existing Cross-Section



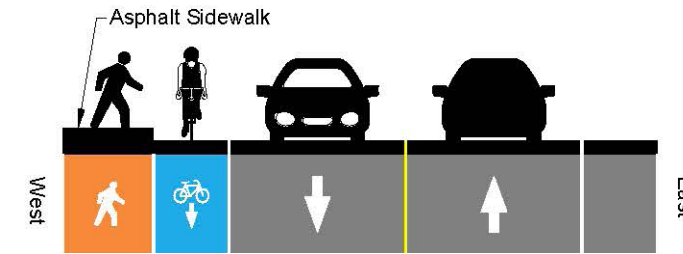
Proposed Cross-Section



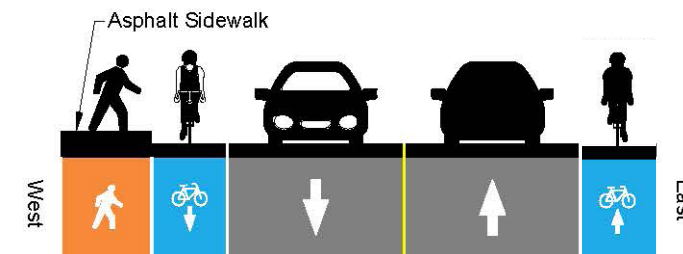
1D-2 From Rainbow Road to Upper Ganges Road (Interim)

In the interim, the shoulder on the east side is formalized into a painted bicycle lane (with pavement markings and signage). Implementing this change would result in people walking no longer having access to the shoulder, requiring that they use the sidewalk on the west side of the road.

Existing Cross-Section



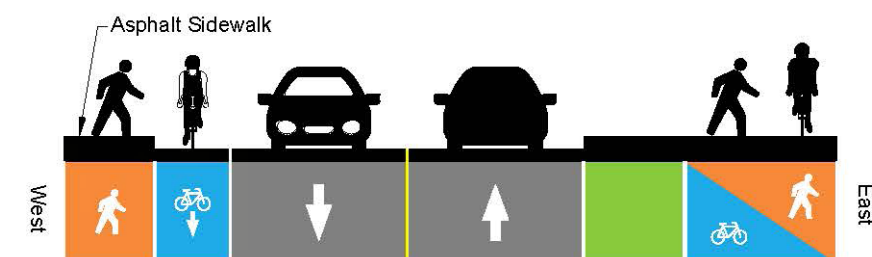
Proposed Cross-Section



1D-3 From Rainbow Road to Upper Ganges Road (Ultimate)

In the longer term, as part of enhancing overall safety and connectivity to/from Ganges Village, a more ultimate option should be pursued as shown below, which shows a multi-use pathway along the east side of the road. This would represent a much more significant capital cost and require property acquisition and retaining walls. This future facility also presents an opportunity to connect with the future Ganges Harbour Walk project, which can make the waterfront more accessible.

Proposed Cross-Section



Action 1E Adopt Infrastructure Design Guidelines

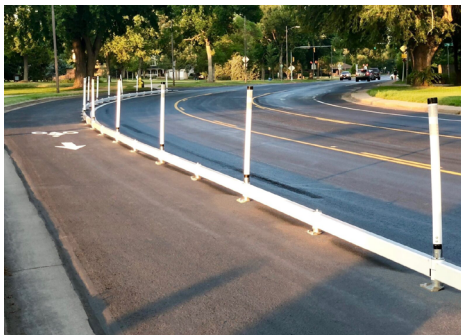
As outlined above, several different facilities are proposed for the main corridors in Ganges Village. This section presents an overview of the infrastructure design guidelines for the proposed facilities.



Adaptive Sidewalk
Adaptive sidewalks are a facility type that have been piloted and implemented in various jurisdictions. They are often used where there is no existing sidewalk or where it may be more challenging to construct a raised concrete sidewalk due to physical or budgetary constraints. They typically include a separation treatment such as rubber curbs, flexible delineator posts, or something more permanent in nature such as bollards.



Multi-use Pathways
Multi-use pathways (MUPs) are typically off-street pathways that are separated from motor vehicle traffic and can be used by any active transportation user, including people walking, cycling, and rolling. MUPs typically accommodate bi-directional travel and are commonly shared spaces. Separation between people walking and rolling, and people cycling may be considered if there is a high volume of users and/or there have been conflicts between active transportation user groups.



Protected Bicycle Lane (Uni-directional)
A designated lane for people cycling and other active transportation users that is physically separated from motor vehicle traffic and people walking and rolling. Uni-directional facilities are the most suitable facility for the Ganges Village context. According to the BC Active Transportation Design Guide (BCATDG), uni-directional facilities should have a minimum width of 1.8m with a desirable width of 2.5m.



Painted Bicycle Lane (including buffer)
Separate travel lanes designated exclusively for bicycle use that are delineated by a painted line and, in some cases, a painted buffer area. What makes the facility buffered is the additional separation between the bicycle lane and the motor vehicle travel lane and/ or parking lane by way of an additional white longitudinal line that runs parallel to the bicycle lane.

Design Guidance for Proposed Facilities

Facility Type	Intended for	Design Details
Adaptive Sidewalk	Walking, Cycling, Rolling	Width – 1.8-2.0m (desirable), 1.5m (constrained) Surface Material – Asphalt provides a smooth surface that is accessible for all user groups. Separation Treatment – Flexible delineator posts, rubber curbs, and/or raised landscaped median.
Multi-use Pathway	Walking, Cycling, Rolling	Width – 3.0m (desirable), 2.7m (constrained) Surface Material – Asphalt provides a smooth surface that is accessible for all user groups. Signage – Shared pathway sign (MUTCDC RB-93), which indicates that both people walking and cycling are allowed to use this facility.
Protected Bike Lane	Cycling	Width – 2.0m (desirable), 1.5m (constrained) Separation Treatment – Flexible delineator posts, precast curbs, and/or raised landscaped median. Signage – Reserved bicycle lane sign (MUTCDC RB-90, RB-91) should be placed along protected bike lanes. The reserved bicycle lane ends sign (MUTCDC RB-92) should be placed where bike lanes end.
Painted Bike Lane	Cycling	Width – 2.0m (desirable), 1.5m (constrained) Buffer – 0.6m (desirable), 0.3m (constrained)

More detailed guidelines for cycling facilities are found in Chapter D of the BC Active Transportation Design Guide

Action 1F Connect to Active Transportation Network Outside of Ganges Village

The recommended corridor improvements within Ganges Village will enhance safety, connectivity, and overall accessibility for active transportation users once implemented. However, these improvements alone will not be sufficient to facilitate active transportation travel to/from Ganges Village. As such, the CRD, in collaboration with MOTI, will need to continue to identify, prioritize, and implement improvements along the island’s major roadways to help meet the vision and guiding principles of the ATNP and the recommendations in the Salt Spring Island Cycling Safety Review.

The Salt Spring Island Cycling Safety Review identifies the lack of continuous cycling facilities along the island’s three major corridors including Vesuvius Road, Lower Ganges Road, and Fulford-Ganges Road. The report identifies several cycling facility options for these corridors and ultimately recommended that one option be pursued in the short-term—bicycle accessible shoulders. The BC Active Transportation Design Guide recommends an absolute minimum shoulder width of 1.2 metres based on the horizontal operating envelope of a person cycling. However, the authors of the Spring Island Cycling Safety Review note that an absolute minimum shoulder width of 1.0 metres would be supported given the rural nature of Salt Spring and other physical constraints including the limited right-of-way.

In alignment with the Spring Island Cycling Safety Review, it is recommended that the CRD, in collaboration with MOTI, prioritize the following to enhance connectivity to Ganges Village. Implement bicycle accessible shoulders along the following segments:

- 1. Fulford-Ganges Road (from Beddis Road to Seaview Avenue)
- 2. Lower Ganges Road (from Booth Canal Road to Atkins Road)

Actions & Indicators Summary

Strategy Area 1 Actions:	
1A	Implement McPhillips Avenue pedestrian facilities
1B-1	Implement Rainbow Road pedestrian and cycling facilities (from Jackson Road to Lower Ganges Road)
1B-2	Implement Rainbow Road pedestrian and cycling facilities (from Kanaka Road to 166 Rainbow Road)
1B-3	Implement Rainbow Road pedestrian and cycling facilities (west of Windsor Plywood)
1C-1	Implement Fulford-Ganges Road pedestrian and cycling facilities (from Seaview Avenue to Lower Ganges Road)
1C-2	Implement Fulford-Ganges Road pedestrian and cycling facilities (from Lower Ganges Road to Purvis Lane)
1D-1	Implement Lower Ganges Road pedestrian and cycling facilities (from Fulford-Ganges Road to Rainbow Road)
1D-2	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Interim Solution)
1D-3	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Ultimate Solution)
1E	Adopt infrastructure design guidelines
1F	Connect to active transportation network outside of Ganges Village
Measuring the Actions	
Total length (metres) of new pedestrian facilities	
Total length (metres) of new cycling facilities	
The number of pedestrian and cycling collisions	

6.0 Strategy Area 2

Make Ganges Village Accessible for all Residents and Visitors

Strategy area 2 includes a total of 6 actions that can be pursued to improve accessibility and road safety and help Ganges become a more pedestrian friendly village. Most of Salt Spring Island's sidewalk network is concentrated around Ganges Village, however, many sidewalks and curb letdowns in the Village are not accessible for the elderly, those using strollers / baby carriages, and those with mobility aids.

Most of Salt Spring Island's sidewalk network is concentrated around Ganges Village, however, many sidewalks and curb letdowns in the Village are not accessible for the elderly, those using strollers / baby carriages, and those with mobility aids.

There are many examples of pedestrian facilities on the island with deficient curb ramps, also referred to as "curb cuts" and "sidewalk letdowns". They are required for people using wheelchairs, power scooters, and other mobility devices, but also benefit people with strollers, baggage, and delivery carts. They are also used as a navigational tool by people with visual impairments. Many of the curb ramps on the island do not meet best practices in the BC Active Transportation Design Guide.

Accessibility is a particular challenge within Ganges Village, where surface parking lots, haphazard parking during peak tourism months, and access to businesses and store fronts present accessibility challenges. Additionally, Most of the sidewalks or pathways have insufficient widths for those using a mobility aid to pass safely and comfortably.

The following key themes emerged from public engagement and technical analysis:

1. Missing pedestrian connectivity to key destination
2. Missing or insufficient curb ramps
3. Existing pedestrian facilities have uneven surfaces
4. Narrow or obstructed pedestrian through zones for passing or mobility aids
5. On-street parking overhanging and obstructing the pedestrian through zone
6. Violations of no-parking zones obstructing curb ramps or pedestrian through zone
7. Insufficient pavement/gravel conditions along sidewalks/shoulders



What We Heard

"We need proper sidewalks and ramps to accommodate our disabled community members, parents with strollers and the elderly."

"As a pedestrian the harbour area and Ganges village is very unfriendly. The parking patterns are all over the place and traffic patterns as well. Please consider closing down some routes from vehicles altogether - and encourage the culture of a car free community."

Action 2A Conduct a Land Ownership Survey of Ganges Village

A major challenge in addressing active transportation infrastructure improvements and accessibility challenges in Ganges Village is the fragmented nature of jurisdictional boundaries and ownership. The Capital Regional District and the Local Community Commission cannot continue to rely on property owners to address accessibility barriers along their frontages. To identify missing, substandard, or insufficient pedestrian infrastructure, the CRD—with its associated partners—must conduct a comprehensive land ownership survey of Ganges Village to identify the ownership of properties, including those owned by the Ministry of Transportation Infrastructure, private owners, and commercial entities.

A land ownership survey would allow the CRD to undertake a more formal assessment of the condition of the sidewalks, pathways, crosswalks, and curbs to determine legal responsibility, resource requirements, and subsequently be in a position to develop design specifications and maintenance standards. Specifically, it is recommended that the CRD investigate options related to tenure as part of completing the land ownership survey.

This action would serve as an important first step in addressing accessibility challenges in the village, while reducing the reliance on property owners to undertake such improvements.



Action 2B Adopt a 30 km/h Posted Speed Limit in the core of Ganges Village

Throughout the community engagement process, the issue of high vehicle speeds emerged as one of the top barriers for walking and cycling on Salt Spring today. Currently, Ganges Village has a 50 km/h posted speed limit. Reducing the posted vehicle speed limit—and thereby reducing vehicle speeds—can be effective at improving overall road safety by preventing crashes and reducing the severity of injuries.

Significant safety concerns can arise due to high operating speeds and differential speeds between drivers and vulnerable road users such as pedestrians and cyclists. Motor vehicles moving at high speeds pose a greater risk to vulnerable road users, who are at a heightened risk of severe injury or even death in the event of a collision. When drivers are traveling at lower speeds, they have more time to react to unexpected events or hazards in the road. This can help them avoid collisions with pedestrians or cyclists who may suddenly enter the roadway.

One way of achieving lower speeds is through adopting a 30 km/h posted speed limit within Ganges Village. A 30 km/h zone can create a more pleasant and pedestrianized village environment for residents and visitors alike. Further, there are several co-benefits associated with reduced operating speeds including reduced noise levels and a reduction in collisions.

The CRD should work closely with MOTI to create a 30 km/h posted speed limit within the core of Ganges Village to improve both pedestrian and cycling safety, which is consistent with the findings and recommendations in the Salt Spring Island Cycling Safety Review. The specific locations and boundary of the 30 km/h posted speed limit zone will need to be determined through conversations with MOTI; however, the zone could comprise most of the village including Drake Road (south) to Upper Ganges Road (north) and Rainbow Road as far west as Atkins Road.

Overall, the successful implementation of a 30 km/h zone in Ganges Village will require installing appropriate signage and road markings to clearly indicate the reduced speed limit as well as supporting an awareness campaign to educate residents and visitors about the new speed limit and the importance of reducing speed in areas with high pedestrian and cycling activity.

What We Heard

In the second survey, we asked the community *“What is your level of support for implementing a 30 km/h zone within the Ganges Village?”*

Over 82% of survey respondents indicated they strongly support or support the implementation of a 30km/h zone within the Ganges Village.

Action 2C Create Village Gateways with Traffic Calming Design Interventions

The lowering of posted speeds should be accompanied by traffic calming design interventions to have a more effective impact in slowing down vehicles. The ATNP envisions “gateways” in and out of the Ganges Village to better signal to drivers that they are entering an area with higher pedestrian activity. Two gateways to Ganges Village are recommended at the following locations:

1. Fulford-Ganges Road / Seaview Avenue (southern gateway)
2. Lower Ganges Road / Rainbow Road (northern gateway)

Specifically, the following treatments are recommended for these two gateways:

- Pedestrian refuge island, which reduces the exposure time of a person walking in the intersection. They narrow the roadway and can help increase overall comfort and safety for people walking.
- Curb extensions can visually and physically narrow the roadway. They help create safer and shorter crossings for people walking. They also increase the overall visibility of people crossing the roadway by aligning them with the parking lane and reducing overall crossing distance.
- 30 km/h signage would be posted at the start of the gateways.
- Pedestrian flashers, which would enhance warning and awareness for motorists of a crosswalk.
- All the proposed treatments above are conceptual in nature; the exact traffic calming treatment will be determined in the detailed design stage once these intersections are rebuilt.



Examples of pedestrian refuge islands in Esquimalt (top) and Crofton (bottom).

What We Heard

In the second survey, we asked the community *“What is your level of support for implementing traffic calming measures at the gateways entering Ganges Village?”*

Over 84% of survey respondents indicated they strongly support or support the implementation of traffic calming measures at the Village gateways.

Action 2D Undertake Reviews of Intersections within Ganges Village

As outlined in the BC Active Transportation Design Guide, intersection design is a critical part of both pedestrian and cycling facility design. The provision of safe and accessible crossings makes it easier for people of all ages and abilities to move around their community to reach their destination. Similarly, active transportation network planning must consider how people cycling can navigate intersections in a safe and comfortable manner.

Based on the public engagement feedback and the technical analysis completed in the development of the ATNP, the CRD, in collaboration with MOTI, should undertake intersection improvement reviews that could lead to changes related to signage, pavement markings, intersection geometry, vehicle speeds, and the elimination of conflicts with other roadway users. For example, this could include the installation of Rectangular Rapid Flashing Beacons at uncontrolled approaches or protected cycling infrastructure up to the edge of the intersection.

The following locations are recommended for a review. More detailed guidance on intersection and crossing treatments is available in Chapter G of the BC Active Transportation Design Guide.

2D-1 Fulford-Ganges Road and Lower Ganges Road

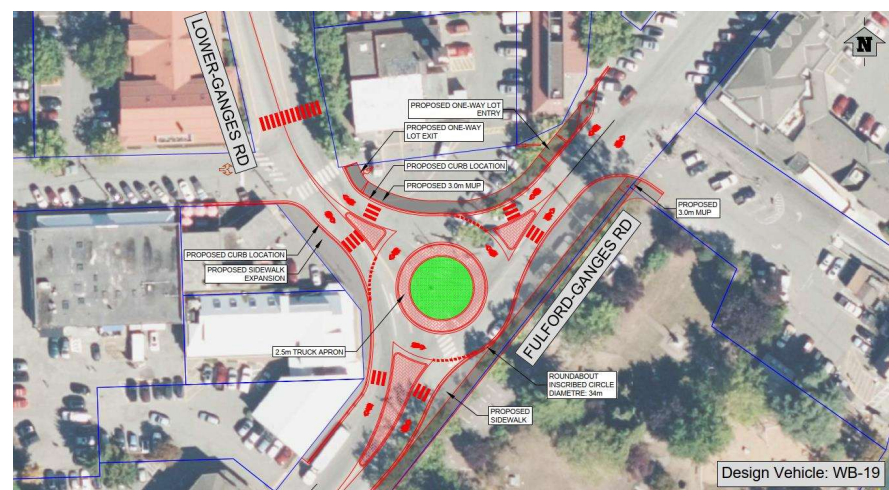
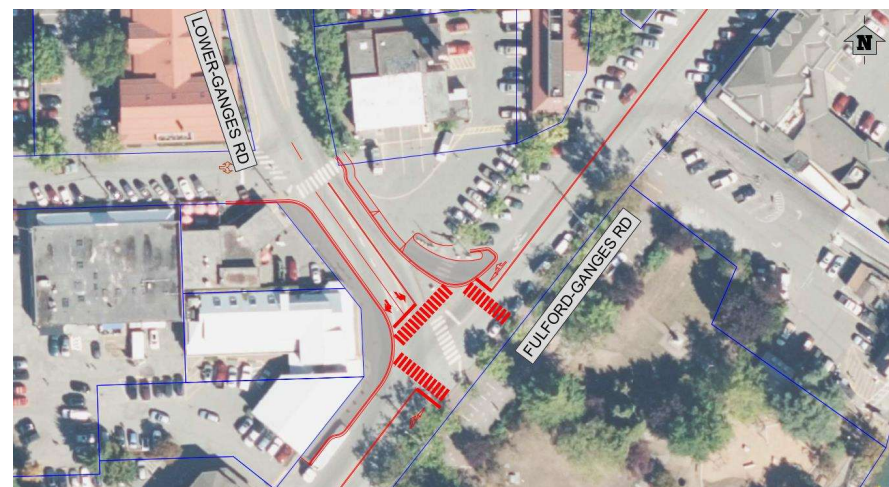
This intersection serves the primary movement of cars to and from Lower Ganges Road to Fulford-Ganges Road. The current configuration of the intersection is complex with unclear right-of-way and accessibility challenges.

Two concepts were developed as part of the Salt Spring Island Cycling Safety Review. In the interim, it is recommended that the intersection be realigned to a 90-degree angle with stop signs and crosswalks on all legs. In the long run, an alternative proposal suggests constructing a single-lane roundabout, but this would depend on the planned relocation of the fire hall that is situated near the current intersection.

Existing Conditions



Recommendations from the Salt Spring Island Cycling Safety Review



2D-2 Fulford-Ganges Road and Purvis Lane

Immediately adjacent to parking, this intersection has poor pedestrian visibility, poor sightlines, and unclear right-of-way. On-street parking near the crosswalk should be potentially removed to address these issues.

2D-3 Lower Ganges Road and McPhillips Avenue

Diagonal marked crosswalk with no dedicated pedestrian facility, which presents accessibility challenges for those with mobility aids. When the proposed corridors improvements are made to Lower Ganges Road, specific intersection treatments that could be implemented at this location including conflict zone markings and signage to indicate limited sightlines.

2D-4 Lower Ganges Road and Herford Avenue

Example of a location with deficient curb ramps. Where feasible, the recommended approach per the BC Active Transportation Design Guide is to provide double curb ramps. Double curb ramps help to provide full universal access by landing pedestrians directly in the crossing area and in the desired direction of travel, rather than entering the road at an angle and having to reorient themselves. This is especially important for pedestrians using mobility devices and who are visually impaired.

2D-5 Lower Ganges Road and Rainbow Road

This location is proposed as one of the "gateways". The intersection currently has a pedestrian-activated amber flasher. Due to its proximity to Salt Spring Elementary School, it should be a candidate for additional safety improvements including traffic calming measures such as a pedestrian refuge island with curb extensions. It is important to consider pedestrian safety for the Southern Gulf Island school children arriving by water taxi at Ganges who are walking from the docks to Rainbow Road across Lower Ganges Road.



2D-6 Lower Ganges Road and Upper Ganges Road

This intersection is located at the bottom of a hill on Lower Ganges Road with long crossing distances and limited pedestrian visibility. The Salt Spring Island Cycling Safety Review recommends realigning the location of stop bar and adding Rectangular Rapid Flashing Beacon or similar device for the crosswalk across Lower Ganges Road to improve pedestrian visibility and safety.

Existing Conditions



Recommendations from the Salt Spring Island Cycling Safety Review



Action 2E Retain and Retrofit Accessible Parking within the Village

As outlined in Strategy Area 1, several of the recommended corridor improvements would result in the loss of some on-street parking in exchange for active transportation facilities. This is a tradeoff and something many other communities in BC are having to contend with. In the Ganges Village context, where there is an opportunity to achieve a pedestrianized village, utilizing parking space for active transportation is a cost-effective solution compared to moving curbs.

However, the removal of all on-street parking is not practical as some users require parking at their destination. As such, all existing accessible parking spaces should be retained (where possible). By retaining accessible parking as part of the Salt Spring Island Active Transportation Network Plan, the community can ensure that people with mobility impairments have access to buildings and services in the community. This is particularly important given the island's aging population and can include those who:

- need the extra width of the designated spaces to get in and out of their vehicles in a wheelchair;
- use crutches, a cane or other mobility aid, or
- need to park close to a building entrance because their health prevents them from walking very far.

Retaining and providing accessible parking options as part of the Active Transportation Network Plan is an essential step towards creating a truly inclusive and accessible community. Therefore, it is recommended that the CRD first complete an inventory of the accessible parking stalls within the village. Then, it should ensure that all spaces be upgraded to a higher standard as they do not meet best practices. The following design standards are recommended:

A. Design & Layout

- The length and width of the stall should have the same dimensions as a regular on-street stall. However, a no parking zone (access aisle) should be provided behind all accessible stalls and be a minimum of 1.8m in width.
- A curb ramp should align with the no parking zone (access aisle) to ensure there is an accessible path of travel from the road to the sidewalk.
- A clear 2m sidewalk/boulevard space should be provided adjacent to the side doors to make it easier for wheelchair users to enter/exit their vehicle using a side lift ramp.

B. Curb

- All accessible spaces should have a blue painted curb.

B. Signage

- A standalone vertical sign should be installed at each standard and van accessible parking space consistent with best practices.

D. Pavement Marking

- The pavement marking should have a blue background with the new International Symbol of Access in white for high tonal contrast.



Action 2F Establish No-parking Zones with Physical Barriers

Parking restrictions are intended to minimize an unwanted behaviour. For example, restrictions may be in place to prevent vehicles from blocking driveways, encroaching pedestrian facilities, and minimizing safety hazards. In areas where unregulated parking is an accessibility concern, it is essential to formalize no-parking zones with physical barriers, such as bollards or curbs, to prevent vehicles from parking in prohibited areas.

Due to the CRD’s limited ability to enforce parking within Ganges Village, it is recommended that they work with MOTI to create no-parking zones whereby specific on-street parking spaces would be marked off with physical barriers to prevent vehicles from blocking these areas, which would reduce the risk of accidents, injuries, and other safety concerns.



Actions & Indicators Summary

Strategy Area 2 Actions:	
2A	Conduct a land ownership survey of the Ganges Village
2B	Adopt a 30 km/h posted speed limit in the core of Ganges Village
2C	Create village gateways with traffic calming design interventions
2D	Undertake reviews of intersections within Ganges Village
2E	Retain accessible parking within the Village
2F	Establish no-parking zones with physical barriers where necessary
Measuring the Actions	
Number of traffic calming devices installed	
Number of crashes resulting in serious injury or death of people who are walking or cycling	
Number of curb ramps meeting accessibility design guidelines	
Number of accessible parking spaces that meet best practices	

7.0 Strategy Area 3 Encourage Sustainable Modes

In addition to the provision of appropriate infrastructure, a high-quality active transportation network must be supported by community programs, educational initiatives, amenities, maintenance, and policies to help facilitate culture change. This section outlines supportive active transportation initiatives and programs that the CRD could lead and/or coordinate with other organizations on Salt Spring Island.

Action 3A Develop a Bike Parking Retrofit Program

Ganges Village is the commercial hub of Salt Spring Island, with a variety of shops, restaurants, and attractions. However, the lack of dedicated and high-quality bicycle parking facilities in this area can be a major deterrent for people cycling, as it can be challenging to find a safe and secure place to lock up their bikes.

Bicycle parking is currently available in the village, but the racks are often poorly maintained or located in inconvenient locations. Most of the racks are “coat hangers”, which are not recommended in the BC Active Transportation Design Guide (BC ATDG) as they do not accommodate all types of bicycles, especially oversized bicycles including electric cargo bikes and bikes with trailers. According to the BC ATDG, the ‘Inverted U’ and ‘Post and Ring’ rack types are recommended due to their versatile application. They can also accommodate oversized bicycles, but sufficient space is required to allow people cycling to park without obstructing pedestrian facilities or blocking access to private properties. Over time, there may other forms of micro-mobility on the island including e-scooters and therefore other parking design details will need to be considered such as proximity to electrical outlets for charging.

To help achieve better designed bike parking facilities, it is recommended that the CRD establish a bike parking retrofit program. The purpose of the program is two-fold: (1) allow the CRD to retrofit, over time, all its CRD-owned short-term bike parking facilities to align with the BC ATDG and (2) work with business and commercial property owners to encourage them to install or retrofit short-term bike parking to align with best practices. The CRD will need to explore different incentive structures to help get buy-in from the private sector, which could include tax breaks or other financial mechanisms.

Additional Information

More detailed parking guidelines for oversized bicycles and e-scooters are found in Chapter H.5 of the BC Active Transportation Design Guide.

Bicycle Racks for All Applications

Inverted U

(Also called loop or staple rack)



Post and Ring



Image credit: BC Active Transportation Design Guide



Example of ‘Coat Hanger’ rack type which does not meet best practices



Image credit: Dublin City Council

Action 3B Identify Opportunities to Increase Off-Street Parking

The implementation of new active transportation facilities will require the reallocation of road space and losses in on-street parking. Several on-street parking spaces will be lost once all the active transportation facilities are constructed. To mitigate these impacts, it is recommended that the CRD secure off-street parking facilities within Ganges Village to increase parking supply for market vendors, staff, visitors, and residents outside of the congested areas.

The CRD should undertake engagement and consultation with private and public entity landowners to determine the feasibility of using these areas for parking and specifically consider factors such as location, size, accessibility, and safety.

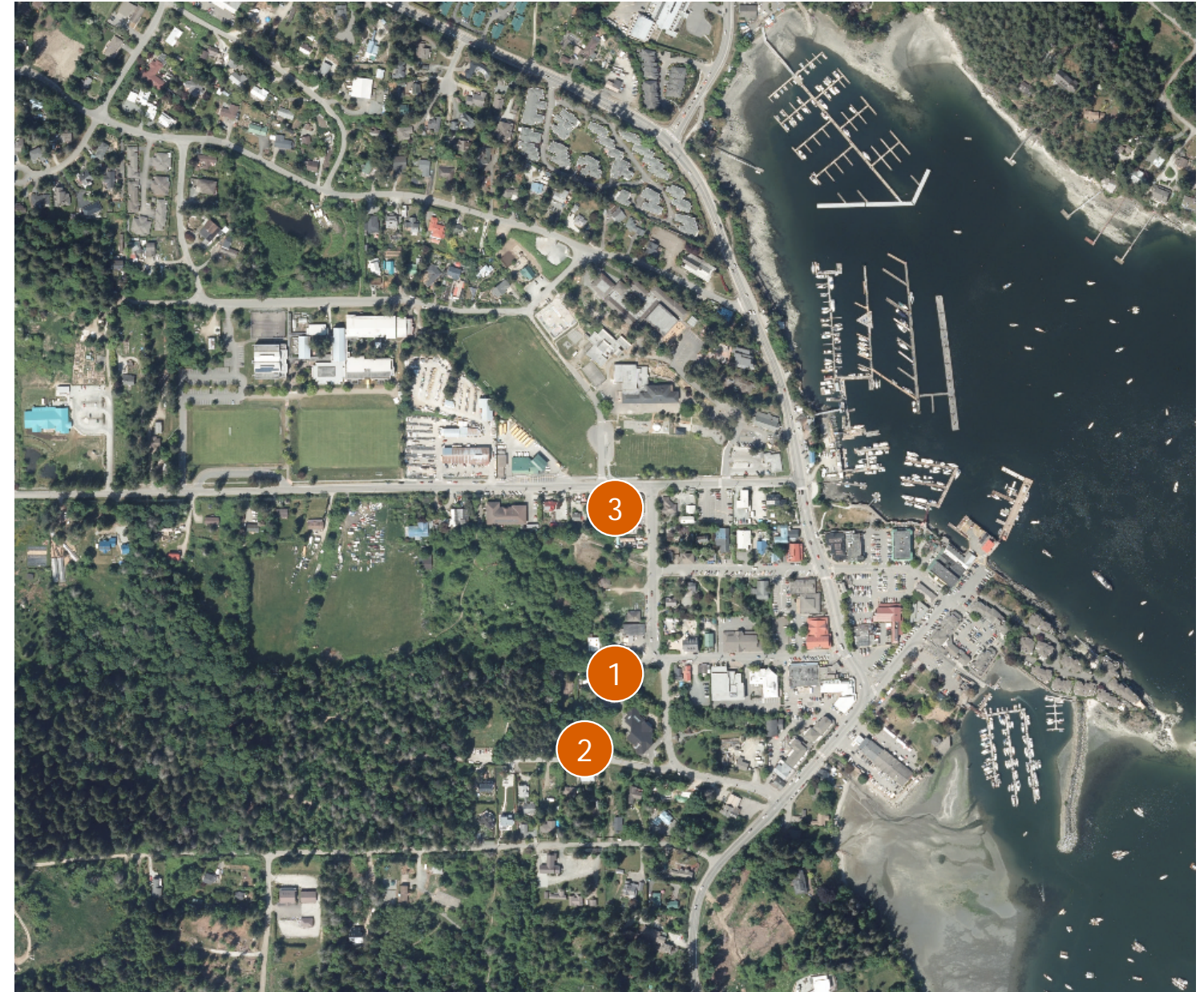
As shown in the map, there are three potential locations for dedicated off-street parking facilities that could be used as general parking for the village:

1. A row of parking stalls north of ArtSpring
2. The off-street lot immediately west of ArtSpring
3. A private lot along Rainbow Road near Jackson Avenue

The parking at ArtSpring could be utilized for off-street parking either on a temporary or permanent basis. It is recommended that the CRD negotiate with ArtSpring for the use of the off-street lots. Specifically, the CRD can discuss the possibility of leasing or renting these spaces for parking on “peak” parking demand day and agree to terms and conditions that include operational responsibility and cost. Further, the CRD should argue that both parking locations would facilitate a connection to the proposed improvements on McPhillips Avenue (Action 1A), which would provide more direct and safe connections for pedestrians trying access the core of the village.

A third location along Rainbow Road is also recommended, which would allow people to utilize the future active transportation facilities along this corridor to access various parts of the village.

Securing additional off-street parking would create more capacity and alleviate existing demands on on-street parking and the off-street lots in the village. This would help to alleviate parking congestion and traffic flow while making it easier for drivers to find available parking spaces. This would also help to better utilize the existing parking supply in Ganges Village and enhance the overall visitor experience.



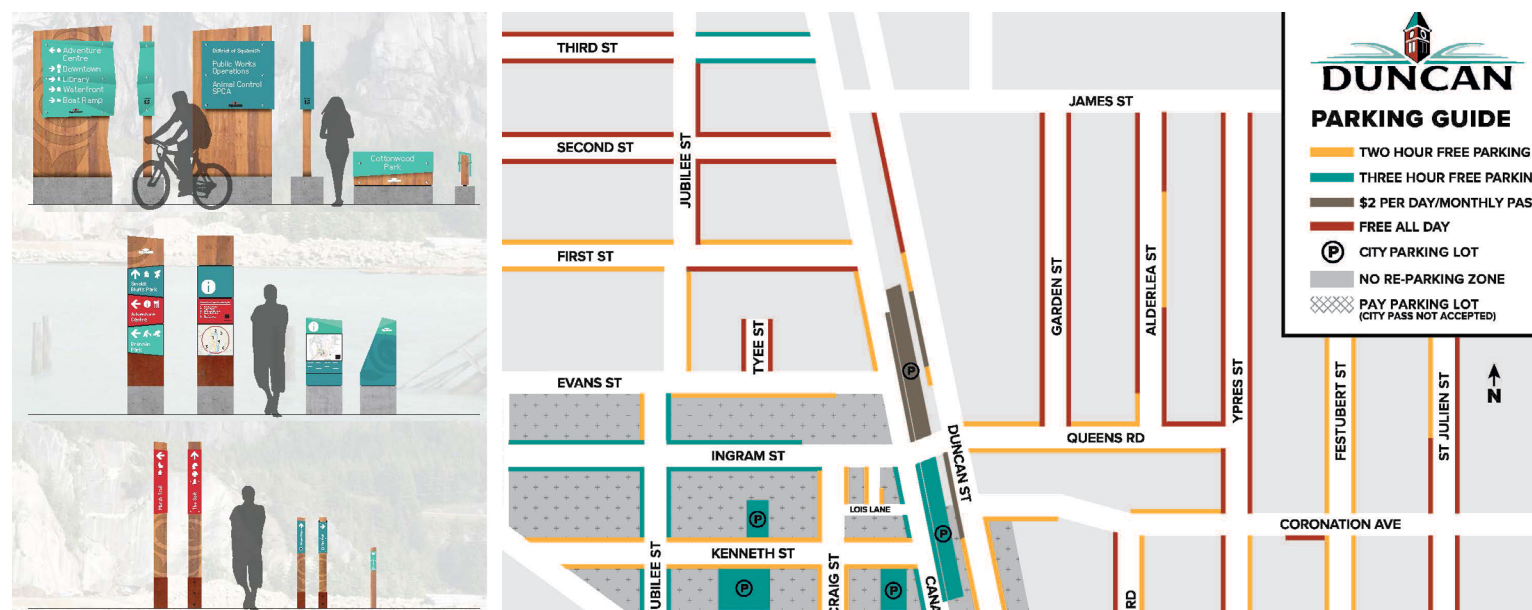
Action 3C Develop a Parking Wayfinding Program

According to the 2018 Ganges Parking Study, there are approximately 1,156 parking spaces within the “Village Core”, which makes up most of the study area for the ATNP. Even though there are a significant number of parking spaces, visitors, especially on market days, may not know where to find parking. One way to address this challenge is to develop a parking wayfinding plan.

Developing a parking wayfinding plan is a crucial step towards optimizing parking in Ganges Village. Finding parking can be a frustrating experience for visitors and can negatively impact their overall experience in Ganges Village. By reducing the number of cars circling or “cruising” for an available parking space, overall traffic operations would improve, which would thereby reduce congestion and travel times. This would create a more efficient and safer environment for all road users.

It is recommended that the CRD determine the parking areas that will be included in the wayfinding plan and assess their signage needs, considering the location, accessibility, and capacity of each area. A comprehensive wayfinding system should be developed that includes maps, directional signs, and other visual cues to guide drivers to available parking areas. It is important to ensure that the system is user-friendly, easy to follow, and clearly communicates critical information such as time limits.

It is important to regularly evaluate the effectiveness of the parking wayfinding plan and make adaptations as necessary to ensure that it continues to meet the needs of the community over time.



Example of wayfinding / signage and a parking guide map

Action 3D Work with BC Transit to Integrate Active Transportation Modes

Public transit is considered a form of active transportation as all transit trips begin and end with a person walking, cycling, or rolling. Therefore, better integrating transit with the active transportation network is essential for increasing overall active transportation mode share. Integration refers to the coordination and connectivity of different modes of transportation, including public transit, walking, and cycling. BC Transit provides bike racks on their buses to facilitate people to easily combine active transportation with public transit.

When transit systems are integrated with active transportation options, it becomes easier for people to combine different modes of transportation to reach their destinations or provide the option to transit part of the way or one-way to their destination. Transit also provides a weather-proof option for supporting active transportation by providing an alternative option in inclement weather such as rain or snow, people can choose to use public transit instead of walking or cycling to their destination.

One of the infrastructure priorities identified in the Salt Spring Island 2021 Transit Future Service Plan is to create two additional bays at the Ganges Exchange to accommodate increased peak service. As part of adding these bays to the Ganges Exchange, the CRD should work with BC Transit to ensure that additional amenities are included that support active transportation such as short-term bicycle parking racks, a bus shelter, and wayfinding / signage to direct transit users to key destinations in Ganges that are within walking distance.



Actions & Indicators Summary

Strategy Area 3 Actions:	
3A	Develop a bike parking retrofit program
3B	Identify opportunities to increase off-street parking
3C	Develop a parking wayfinding program
3D	Work with BC Transit to integrate active transportation modes
Measuring the Actions	
The percentage of bicycle parking racks within Ganges Village meeting best practices	
The number of new bicycle parking racks installed	
The number of parking wayfinding signs installed	
The number of off-street parking stalls added	

This Page Intentionally Left Blank

8.0 Implementation Strategy

8.1 Big Moves

A total of 26 actions are recommended in the ATNP. The implementation of each action will require time, resources, and financial commitments. The recommended sequencing of the actions is shown in Section 8.3 (Action Plan). While all the actions in the ATNP are critical for helping meet the plan’s vision and targets, some are deemed to be of higher importance to have a more immediate and direct impact on Ganges’ active transportation network.

The five big moves were selected based on meeting at least one of the following criteria:

1 Low-cost

In alignment with the first guiding principle of the ATNP, a big move should be relatively low cost recognizing the limited financial resources for infrastructure on the island.

2 Actionable

In alignment with the first guiding principle of the ATNP, a big move should be actionable, meaning that it requires minimal staff time and strong support from key stakeholders such as MOTI to make implementation seamless.

3 High Impact

High impact refers to increasing the number of people walking, cycling, and rolling while improving safety and accessibility for the most vulnerable road users.

The following “big moves” have been identified with their corresponding action number. The big moves have been listed based on their order of priority reflecting the criteria above. Ultimately, however, the CRD and its partners will need to determine what is most feasible to implement first based on funding opportunities, staffing, and overall resources.

8.1.1 Big Move 1

Adopt a 30km/h posted speed limit in the core of Ganges Village (Action 2B)

What is the move?

Per Action 2B, the CRD should work closely with MOTI to create a 30 km/h posted speed limit within Ganges Village to improve both pedestrian and cycling safety, which is consistent with the findings and recommendations in the Salt Spring Island Cycling Safety Review.

Why is it important?

A 30 km/h posted speed limit within Ganges Village will help contribute to lower speeds and improved safety for active travelers while waiting for all the recommended active transportation facilities to be built. While changing the posted speed limit is an important first step to address speeding and road safety, additional traffic calming measures (Action 2C) will also be needed to address vehicle operating speeds.

8.1.2 Big Move 2

Implement McPhillips Avenue Pedestrian Facilities (Action 1A)

What is the move?

Per Action 1A, McPhillips Avenue is recommended to be upgraded with pedestrian facilities to help create a more pedestrian village in Ganges.

Why is it important?

The recommended corridor improvements to McPhillips Avenue are anticipated to improve pedestrian safety and increase overall walkability within the village. McPhillips Avenue serves as an important east-west corridor and improved pedestrian facilities will make it easier for people walking and rolling to access destinations along McPhillips and improve access to the core of the village. McPhillips does not experience the volume of vehicle and bicycle trips or transit activity as Lower Ganges and Fulford-Ganges Road and therefore has great potential to be implemented as a quick-win project to enhance active transportation infrastructure.

8.1.3 Big Move 3

Improve the Parking Experience in Ganges Village (Actions 2E, 3B, 3C)

What is the move?

Per Action 2E, 3B and 3C, vehicle parking will continue to be an important amenity within Ganges Village. The loss of on-street parking associated with active transportation facilities presents an opportunity to improve existing accessible on-street parking and increase the supply of off-street parking.

Why is it important?

Some user groups, including seniors and persons with mobility impairments, require vehicle parking closer to their destination. As such, retaining and retrofitting existing accessible parking spaces is critical to meet their needs.

Similarly, the provision of more off-street parking can serve the needs of multiple user groups including those who may only require parking for a short period of time (e.g., customers, visitors), or a longer period (e.g., vendors, businesses, employees).

The objective of this Big Move is to enhance the parking experience in Ganges Village to alleviate community concerns about the ATNP's effects on parking. By doing so, it aims to increase community buy-in and support for the proposed active transportation facilities that require the removal of on-street parking spaces.

8.1.4 Big Move 4

Conduct a Land Ownership Survey of Ganges Village (Action 2A)

What is the move?

Per Action 2A, a major challenge in addressing active transportation infrastructure improvements and accessibility challenges in Ganges Village is the fragmented nature of jurisdictional boundaries and land ownership.

Why is it important?

A land ownership survey would allow the CRD to undertake a more formal assessment of the condition of the sidewalks, pathways, crosswalks, and curbs to determine legal responsibility, resource requirements and develop design specifications and maintenance standards. It would represent an important first step in addressing accessibility challenges in the village.

8.1.5 Big Move 5

Implement Fulford-Ganges Road Pedestrian and Cycling Facilities from Seaview Avenue to Lower Ganges Road (Action 1C)

What is the move?

Per Action 1C-1 and 2C, new active transportation facilities are recommended for this corridor including protected cycling facilities on both sides, and an adaptive sidewalk on the south side. Traffic calming improvements are also recommended, including a pedestrian refuge island which would create a "village gateway" on the south end.

Why is it important?

The provision of new active transportation facilities in this area of Ganges Village will help increase pedestrian and cycling safety. It will also enhance connectivity to the larger cycling network south of Ganges Village and connect to the larger network envisioned in the village.

The Fulford-Ganges Road corridor between Seaview Avenue and Lower Ganges Road serves as the southern gateway for Ganges Village and sets the tone for the increased presence of people walking and cycling within the Village. This gateway has been prioritized due to the community's concerns around speeding on Ganges hill entering the Village and safety concerns resulting from haphazard on-street parking during summer market days.

8.2 Capital Planning

8.2.1 The Big Moves

The five “big moves” are recommended to be implemented first. The overall cost for big moves 1, 4, and 5, is anticipated to be relatively low and will depend on several factors such as overall signage requirements, the complexity of the land ownership survey, and whether the CRD acquires land for a new off-street parking location. Based on the Class D cost estimates, the total cost of Big Move 2 (Action 1A) and Big Move 5 (Action 1C) is approximately \$432,000.

The table below provides an order of magnitude (Class D) cost estimate for all of the infrastructure improvements identified in Strategy Area 1. The cost estimates are based on concept level information using unit rates for linear works and improvements. The actual costs for implementation for each project could vary and will be confirmed through additional engagement and the detailed design stage. The total level of investment for the infrastructure projects is approximately \$1,647,000 excluding GST.

Per Action 2E, 3B and 3C, vehicle parking will continue to be an important amenity within Ganges Village. The loss of on-street parking associated with active transportation facilities presents an opportunity to improve existing accessible on-street parking and increase the supply of off-street parking.

Action ID & Project Name		Class D Cost Estimate (2023 \$)
1A	Implement McPhillips Avenue pedestrian facilities	\$154,000
1B-1	Implement Rainbow Road pedestrian and cycling facilities (from Jackson Road to Lower Ganges Road)	\$397,000
1B-2	Implement Rainbow Road pedestrian and cycling facilities (from Kanaka Road to 166 Rainbow Road)	
1B-3	Implement Rainbow Road pedestrian and cycling facilities (west of Windsor Plywood)	
1C-1	Implement Fulford-Ganges Road pedestrian and cycling facilities (from Seaview Avenue to Lower Ganges Road)	\$278,000
1C-2	Implement Fulford-Ganges Road pedestrian and cycling facilities (from Lower Ganges Road to Purvis Lane)	\$122,000
1D-1	Implement Lower Ganges Road pedestrian and cycling facilities (from Fulford-Ganges Road to Rainbow Road)	\$568,000
1D-2	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Interim Solution)	\$128,000
1D-3	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Ultimate Solution)	\$154,000
Total		\$1,647,000

8.3 Operational & Maintenance Considerations

The proposed active transportation facilities within Ganges Village will be required to be maintained regularly. There are several hazards that could impact the ability for someone to use a pedestrian or cycling facility including snow, glass or debris, potholes and cracked surfaces, and leaves, for example. The CRD will need to ensure it has a maintenance program in place for the active transportation network, which could include the following:

- Sweeping and removing gravel, debris, and leaves; trimming adjacent vegetation; and adjusting bollards and other elements related to protected bike lane delineators.
- Clearing and removal of debris and snow in the fall and winter and treat and remove ice or slippery conditions.
- There are also asset management activities, which can include repairing pavement surfaces and other road surface appurtenances such as utility covers; replacing worn pavement markings, signs, and signals; mitigating locations with pooling water or drainage issues; replacing broken delineators; maintaining street and path lighting; and repairing and maintaining equipment that is used to maintain cycling facilities.



8.4 Action Plan

The following table outlines the 26 recommended action items under the three strategy areas:

- 1. Reallocate Street Space for Walking, Rolling, and Cycling within Ganges Village
- 2. Make Ganges Village Accessible for All Residents and Visitors
- 3. Encourage Sustainable Modes

For each of the recommended action items, the following have been identified:

Timeframe: The immediate term refers to 1-3 years; short to medium-term is 3-7 years; and long-term refers to 7-10 years. The following timeframes should be used as a guide, however, the overall prioritization of the action(s) may change over time due to shifting priorities and opportunities.

Partners: The CRD is responsible for most actions but collaboration with other organizations / stakeholders where applicable have been identified below.

Implementation Approach: There are different ways to implement an action including (1) capital project; (2) operational / maintenance; (3) technical study; (4) policy / regulatory; and (5) programming / advocacy.



Action ID & Project Name		Timeframe	Partners	Implementation Approach
Strategy Area 1: Reallocate Street Space for Walking, Rolling, and Cycling within Ganges Village				
1A	Implement McPhillips Avenue pedestrian facilities	Immediate	MOTI	Capital Project
1B-1	Implement Rainbow Road pedestrian and cycling facilities (from Jackson Road to Lower Ganges Road)	Medium-Term	MOTI	Capital Project
1B-2	Implement Rainbow Road pedestrian and cycling facilities (from Kanaka Road to 166 Rainbow Road)	Medium-Term	MOTI	Capital Project
1B-3	Implement Rainbow Road pedestrian and cycling facilities (west of Windsor Plywood)	Medium-Term	MOTI	Capital Project
1C-1	Implement Fulford-Ganges Road pedestrian and cycling facilities (from Seaview Avenue to Lower Ganges Road)	Immediate	MOTI	Capital Project
1C-2	Implement Fulford-Ganges Road pedestrian and cycling facilities (from Lower Ganges Road to Purvis Lane)	Medium-Term	MOTI	Capital Project
1D-1	Implement Lower Ganges Road pedestrian and cycling facilities (from Fulford-Ganges Road to Rainbow Road)	Medium-Term	MOTI	Capital Project
1D-2	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Interim Solution)	Medium-Term	MOTI	Capital Project
1D-3	Implement Lower Ganges Road pedestrian and cycling facilities (from Rainbow Road to Upper Ganges Road, Ultimate Solution)	Long-Term	MOTI	Capital Project
1E	Adopt infrastructure design guidelines	Immediate	MOTI	Capital Project
1F	Connect to active transportation network outside of Ganges Village	Long-Term	MOTI	Capital Project

Action ID & Project Name		Timeframe	Partners	Implementation Approach
Strategy Area 2: Make Ganges Village Accessible for All Residents and Visitors				
2A	Conduct a land ownership survey of Ganges Village	Immediate	Businesses, Land Owners	Technical Study
2B	Adopt a 30 km/h posted speed limit in the core of Ganges Village	Immediate	MOTI	Policy / Regulatory
2C	Create village gateways with traffic calming design interventions	Medium-Term	MOTI	Capital Project
2D-1	Undertake intersection review of Fulford-Ganges Road and Lower Ganges Road	Medium-Term	MOTI	Capital Project
2D-2	Undertake intersection review of Fulford-Ganges Road and Purvis Lane	Medium-Term	MOTI	Capital Project
2D-3	Undertake intersection review of Lower Ganges Road and McPhillips Avenue	Medium-Term	MOTI	Capital Project
2D-4	Undertake intersection review of Lower Ganges Road and Herford Avenue	Medium-Term	MOTI	Capital Project
2D-5	Undertake intersection review of Lower Ganges Road and Rainbow Road	Medium-Term	MOTI	Capital Project
2D-6	Undertake intersection review of Lower Ganges Road and Upper Ganges Road	Medium-Term	MOTI	Capital Project
2E	Retain and retrofit accessible parking within the Village	Immediate	MOTI	Capital Project
2F	Establish no-parking zones with physical barriers	Immediate	MOTI	Capital Project

Action ID & Project Name		Timeframe	Partners	Implementation Approach
Strategy Area 3: Encourage Sustainable Modes				
3A	Develop a bike parking retrofit program	Immediate	Businesses, Land Owners	Programming / Advocacy
3B	Identify opportunities to increase off-street parking	Immediate	Businesses, Land Owners	Operational / Maintenance
3C	Develop a parking wayfinding program	Immediate	N/A	Programming / Advocacy
3D	Work with BC Transit to integrate active transportation modes	Immediate	BC Transit	Programming / Advocacy

8.5 Funding Opportunities

As outlined in Section 8.2, the total cost for the active transportation infrastructure projects is \$1,647,000. As an unincorporated community with limited financial resources, the projected cost of all the projects will be challenging to cover for the Capital Regional District alone. Fortunately, there are many funding opportunities available that could pay for these projects over the 10-year horizon of this plan.

A summary of each funding opportunity is below.

8.5.1 CRD Funding

The CRD Salt Spring Island Transportation service was established in 2007 by Bylaw No. 3438 to:

- Provide a public transportation system on Salt Spring Island;
- Carry out transportation studies;
- Provide for the construction, installation, maintenance and regulation of sidewalks and bicycle paths, parking facilities, pedestrian and safety and traffic calming facilities; and
- Implement transportation demand management programs.

The service is funded through an annual tax requisition. The current Financial Plan (2023-2027) supports investments in active transportation infrastructure investments with approximately 50% of the funding being allocated to capital spending and supplemented with available grant funding. According to the 2023-2027 capital plan, a total of \$909,500 has been allocated to transportation for several improvements including pedestrian pathway improvements, maintenance and equipment—some of which could be allocated to the recommended projects in the ATNP.

8.5.2 BC Active Transportation Infrastructure Grant Program

The B.C. Active Transportation Infrastructure Grants Program offers two grant options for Indigenous governments and local governments, including municipalities, regional districts, and Islands Trust. Specifically, the Active Transportation Infrastructure Grant allows eligible governments to apply for a maximum of two grants if they satisfy the following criteria (based on the 2022 intake):

- Projects previously funded prior to 2022/23, or prior to 2021/22 for projects with budgets over \$1M, must be completed by application submission date.
- Project is part of an active transportation network plan or equivalent.
- Project can begin construction once provincial funding has been announced.
- Projects will be completed by March 2025 (projects under \$1 million) or by March 2026 (projects over \$1 million).
- Projects are open to the public.

The grant program typically requires that projects be “shovel-ready”. The CRD, in collaboration with its partners, could begin to action the infrastructure projects and position itself to apply for funding for the next grant intake (2023-2024), which opens September 1, 2023. The program guidelines provide the specific detail on what constitutes as a “shovel-ready” project, which includes the following:

- The Cost Estimate submitted with the Grant Application must be Class A-C and current or forecasted to proposed construction date.
- All project design work is complete.
- Community consultation is complete.

The province cost-shares to a maximum of \$500,000 per project and the CRD would be eligible for 70% of the provincial funding.

8.5.3 Green Municipal Funds

The Green Municipal Fund (GMF) is a program administered by the Federation of Canadian Municipalities intended to help Canadian communities expand their sustainability initiatives. Since 2000, the GMF has deployed \$900M in financing to 1,250+ sustainability initiatives and a further \$1 billion has been committed to the fund through the Federal 2019 budget.

The specific GMF initiative that is relevant to the Salt Spring context is the “Capital Project Transportation Networks Commuting Options”, which is a combined loan and grant funding program for capital projects that reduce pollution by improving transportation systems and networks. This program covers several topics including bike paths, walking and cycling networks that promote accessibility and safety, and evaluation of active transportation infrastructure, among others.

8.6 Next Steps

Salt Spring Island, and Ganges Village in particular, is a small community with an appetite for change. The ATNP is intended to move the community on a path toward better, safer, and more inclusive active transportation. As a smaller community with limited financial and staff resources, the CRD, and its associated partners, will need to move quickly on the low-cost, quick-build solutions identified in the ATNP. Not everything can or should be implemented tomorrow; however, there are several actions—including the McPhillips Avenue improvements and 30 km/h posted limit within the village—that can be implemented in the near term to improve active transportation for all users.

The ATNP should be a living document and not sit on the proverbial bookshelf collecting dust. It is recommended that the ATNP be revisited in 5 years (2028) to explore what has—and has not—been implemented to date and whether any of the actions in this plan need to be reprioritized.

A more walkable, bike friendly, and accessible Ganges Village will take time, and is anticipated to boost the local economy, lower the community’s GHG emissions, and enhance safety for all.



Pedestrian Facilities
(Sidewalks, Pathways)

Cycling Facilities
(Bike Lanes, Bike
Paths, etc.)

